

## COMMITTEE MEMBERS

### Trustees

Jennifer Cunningham, Vice Chair  
Richard Jay, Member  
Art Sperber, Alternate  
Shaun Carey, Alternate

### Staff Liaison

Randy Carlton, Chief Finance &  
Administration Officer



## PRESIDENT/CEO

Daren Griffin, A.A.E.

## CHIEF LEGAL COUNSEL

Ian Whitlock

## BOARD CLERK

Lori Kolacek

## AGENDA

### Finance and Business Development Committee

Tuesday, July 11, 2023 9:00 AM

Reno-Tahoe International Airport, 2001 E. Plumb Lane, Reno, NV  
Administrative Offices, Second Floor

### Notice of Public Meeting

Meetings are open to the public and notice is given pursuant to [NRS 241.020](#).

This meeting will be livestreamed and may be viewed by the public at the following link:

**Watch on Zoom:** <https://us02web.zoom.us/j/88150587204>; **Webinar ID:** 881 5058 7204

### Accommodations

Members of the public who require special accommodations or assistance at the meeting are requested to notify the Clerk by email at [lkolacek@renoairport.com](mailto:lkolacek@renoairport.com) or by phone at (775) 328-6402.

### Public Comment

Anyone wishing to make public comment may do so in person at the Board meeting, or by emailing comments to [lkolacek@renoairport.com](mailto:lkolacek@renoairport.com). Comments received **prior to 4:00 p.m. on the day before the meeting** will be given to the Board for review and included with the minutes of this meeting. To make a public comment during the Zoom meeting, please make sure your computer or device has a working microphone. Use the "Chat" feature to submit a request to speak. When the time comes to make public comments, you will be invited to speak. Public comment is **limited to three (3) minutes** per person. No action may be taken on a matter raised under general public comment.

### Posting

This agenda has been posted at the following locations:

1. RTAA Admin Offices, 2001 E. Plumb
2. [www.renoairport.com](http://www.renoairport.com)
3. <https://notice.nv.gov/>

### Supporting Materials

Supporting documentation for this agenda is available at [www.renoairport.com](http://www.renoairport.com), and will be available for review at the Board meeting. Please contact the Board Clerk at [lkolacek@renoairport.com](mailto:lkolacek@renoairport.com), or (775) 328-6402 for further information.

**1. CALL TO ORDER**

**2. ROLL CALL**

**3. PUBLIC COMMENT**

**4. MINUTES**

- 4.1 Approval of minutes of the June 6, 2023, Finance and Business Development Committee meeting.

**5. INFORMATION, DISCUSSION AND/OR POSSIBLE ACTION**

- 5.1 New Cargo Development RFP update / option to lease and develop land with AFCO (*information only*)

**6. ITEMS RECOMMENDED FOR APPROVAL BY THE FULL BOARD**

- 6.1 Board Memo # 07/2023-51: Consideration of 12-month extension to the Scheidt & Bachmann USA Maintenance Services Agreement in the amount of \$209,950.87 (*for possible action*)

**7. PROJECT UPDATE PRESENTATIONS**

- 7.1 Administrative Award of Contracts – Revenues  
7.2 Administrative Award of Contracts – Expenditures  
7.3 Financial Reporting Package and Operating Statement

**8. COMMITTEE MEMBER COMMENTS, QUESTIONS, AND REQUESTS**

**9. PUBLIC COMMENT**

**10. ADJOURNMENT**



**COMMITTEE MEMBERS**

**Trustees**

Lisa Gianoli, Chair  
Jennifer Cunningham, Vice Chair  
Richard Jay, Member  
Jenifer Rose, Member  
Art Sperber, Alternate  
Shaun Carey, Alternate

**Staff Liaison**

Randy Carlton, Chief Finance & Administration Officer

**PRESIDENT/CEO**

Daren Griffin, A.A.E.

**CHIEF LEGAL COUNSEL**

Ian Whitlock

**CLERK OF THE BOARD**

Lori Kolacek

**--DRAFT--**

**MINUTES**

**FINANCE & BUSINESS DEVELOPMENT COMMITTEE**

**Date:** Tuesday, June 6, 2023

**Time:** 9:00 a.m.

**Location:** Reno-Tahoe International Airport, 2001 E. Plumb Lane, Reno  
Admin Offices, Main Terminal Building, Second Floor

**1. CALL TO ORDER**

The meeting was called to order at 9:00 a.m.

**2. ROLL CALL**

Roll was taken by the Clerk of the Board. All members were present.

**3. PUBLIC COMMENT**

None.

**4. APPROVAL OF MINUTES**

**4.1 May 16, 2023, Finance and Business Development Committee Meeting**

**Motion:** Move to approve the May 16, 2023, minutes as presented

**Moved:** Jenifer Rose **Seconded:** Richard Jay

**Aye:** Trustees Cunningham, Jay, Rose, Gianoli

**Vote:** Motion passed

**5. INFORMATION, DISCUSSION AND/OR POSSIBLE ACTION ITEMS**

None.

**6. ITEMS RECOMMENDED FOR APPROVAL BY THE FULL BOARD**

- 6.1 Board Memo # 06/2023-37:** Adoption of Reno-Tahoe Airport Authority fiscal year 2023-24 Liability and Property Insurance Program and approval of premiums in the amount not to exceed \$1,482,099 *(for possible action)*

This item was presented by Alex Kovacs, Director of Finance. After discussion and comments, the Committee took the following action:

**Motion:** Move to recommend that this item be presented to the full Board on June 8, 2023, for consideration and approval of the proposed motion: *“Approve the Fiscal Year 2023-24 Liability and Property Insurance Program and authorizes the President/CEO, or his designee, to bind the coverages and pay the FY 2023-24 premiums in the amount not to exceed \$1,482,099”*

**Moved:** Richard Jay                      **Seconded:** Jenifer Rose

**Aye:** Trustees Cunningham, Jay, Rose, Gianoli

**Vote:** Motion passed

- 6.2 Board Memo # 06/2023-38:** Authorization for the President/CEO to execute a three-year extension of the Professional Services Agreement with Government Portfolio Advisors, LLC *(for possible action)*

This item was presented by Alex Kovacs, Director of Finance. After discussion and comments, the Committee took the following action:

**Motion:** Move to recommend that this item be presented to the full Board on June 8, 2023, for consideration and approval of the proposed motion: *“Authorize the President/CEO to execute a three-year extension of the Professional Services Agreement for investment advisory services with Government Portfolio Advisors LLC”*

**Moved:** Richard Jay                      **Seconded:** Lisa Gianoli

**Aye:** Trustees Cunningham, Jay, Rose, Gianoli

**Vote:** Motion passed

- 6.3 Board Memo # 06/2023-39:** Authorization for the President/CEO to execute a Third Amendment to Exclusive Master Development Agreement and Third Amendment to Phase I Ground Lease to include various performance milestones, adjust the annual option payment amount, add further and clarifying breach of a substantial and material obligation language, adjust Fair Market Value adjustment date and include a 5% cap on Fair Market Value adjustment with DP RTA Stead PH 1, LLC *(for possible action)*

This item was presented by Aurora Ritter, Manager of Airport Economic Development. After discussion and comments, the Committee took the following action:

**Motion:** Move to recommend that this item be presented to the full Board on June 8, 2023, for consideration and approval of the proposed motion: *“Authorize the President/CEO to execute a Third Amendment to the Exclusive Master Development Agreement (MDA) and Phase I Ground Lease (PIGL) with DP RTA Stead PH 1, LLC (Dermody) to include various performance milestones, adjust the annual option payment amount, add further and clarifying breach of a*

*substantial and material obligation language, adjust Fair Market Value adjustment date and include a 5% cap on Fair Market Value adjustment”*

**Moved:** Jenifer Rose                      **Seconded:** Lisa Gianoli

**Aye:** Trustees Cunningham, Jay, Rose, Gianoli

**Vote:** Motion passed

## **7. INFORMATIONAL ITEMS *(provided for reference only)***

**7.1 Administrative Award of Contracts - Expenditures**

**7.2 Administrative Award of Contracts - Revenues**

**7.3 Financial Reporting Package**

There was no discussion on these items.

## **8. MEMBER COMMENTS, QUESTIONS AND REQUESTS FOR AGENDA ITEMS**

Randy Carlton, Chief Finance and Administration Officer, gave an update on the Conrac finance negotiations.

## **9. PUBLIC COMMENT**

None.

## **10. ADJOURNMENT**

The meeting was adjourned at 9:44 a.m.



## BOARD MEMO NO.: 07/2023-51

**Date of Board Meeting:** July 13, 2023

**SUBJECT:** Consideration of 12-month extension to the Scheidt & Bachmann USA Maintenance Services Agreement in the amount of \$209,950.87

---

### STAFF RECOMMENDATION

Staff recommends approval of the Proposed Motion stated below.

### BACKGROUND

#### *The PARCS system*

This agenda item involves the parking and revenue control system (PARCS) at the Reno-Tahoe International Airport.

The PARCS system is composed of hardware and software used to monitor and control public parking revenue in the parking garage and long-term surface parking as well as to provide access control for airport employee parking areas. This system includes equipment and software that supports (1) access at the parking entry plaza, (2) license plate recognition for fraud protection, (3) credit card processing, (4) Pay-On-Foot cash/credit pay stations, (5) exit plaza cashier facilities, and (6) system computer servers and related equipment. The system is responsible for collecting approximately \$17 million in annual revenue. The PARCS systems have been maintained by Scheidt & Bachmann (S&B) technicians since the installation of the original equipment in 2006.

S&B installed a new PARCS system in June 2019 at a cost of approximately \$750,000. The installation included a one year factory warranty and maintenance agreement. Upon the expiration of that first warranty and service agreement, the Reno-Tahoe Airport Authority (RTAA) entered into a renewable twelve-month agreement. The current twelve-month agreement will expire June 30, 2023 which includes warranty coverage for new parts or any after-hours support. The proposed contract includes parts, service, and 24/7 support. As the system is now aging, and with the return of pre-pandemic passenger levels, the equipment requires additional servicing. The RTAA intends to enter into a traditional multi-year warranty and maintenance agreement, similar to the agreements made in past years, next fiscal year.

#### *Scheidt & Bachmann Proposed Maintenance Services*

This maintenance agreement will provide for the following services:

- Dedicated On-site Factory Trained Technician
- 24 Hour a Day / Seven days a week/ 365 Day Emergency Coverage

- Four (4) Hour Maximum Response Time
- Additional Backup from the western region, if needed
- Four (4) instances of scheduled Preventative Maintenance
- Software Patches and Updates
- Parts used during corrective or preventative and replacement of parts due to normal wear and tear

The potential for loss of revenue, should the PARCS system break down, makes this agreement critical. On average, the RTAA receives approximately \$46,000 in revenue per day through the PARCS system. The system is proprietary to S&B, and requires a highly specialized support team to continually upgrade, monitor and maintain the system software and provide training needed to maintain the associated hardware. Without this agreement, Scheidt & Bachmann technicians would be dispatched from Sacramento, with the risk of one-week delays. Furthermore, the RTAA would be responsible for all time and materials for any maintenance request.

From July 1, 2022, through June 30, 2023, there have been approximately 150 service-related calls.

### **DISCUSSION**

This action will provide on-site maintenance services, emergency after hours maintenance services, scheduled preventative maintenance and software updates and patching for the PARCS system from S&B until the end of the 2024 fiscal year.

This action is in support of the Strategic Priority: Financial Stewardship, as adopted in the RTAA Fiscal Year (FY) 2024-2028 Comprehensive Strategic Plan.

Nevada Revised Statute 332.115.1(a) and (c) provide exemptions to competitive bidding requirements for “items that may only be contracted from a sole source” as well as for “additions to and repairs and maintenance of equipment which may be more efficiently added to, repaired or maintained by a certain person” without having to comply with the requirements of a competitive bid. The proposed maintenance agreement qualifies for this exemption. Accordingly, staff is recommending this action be exempt from competitive bidding as permitted by Nevada Revised Statute 332.115.1(a) and (c).

Due to the critical and proprietary nature of these systems, the use of vendor-provided support from S&B is considered a requirement, as an outside vendor providing such maintenance is not a viable option. Appropriate funding for the annual warranty and maintenance has been provided each year in the Operations and Maintenance (O&M) Budget.

### **FISCAL IMPACT**

The PARCS Maintenance Agreement of \$209,950.87 was included in the proposed FY 2023-24 budget.

### **COMMITTEE COORDINATION**

Finance and Business Development Committee

**PROPOSED MOTION**

“Move to authorize the President/CEO to extend the existing Maintenance Services agreement for the Parking Access and Revenue Control System (PARCS) at the Reno-Tahoe International Airport (RNO) with Scheidt & Bachmann USA (S&B), in the total amount of \$209,950.87 for a period of 12 months.”

# Administrative Report

**Date:** July 11, 2023  
**To:** All Board Members  
**From:** Daren Griffin, President/CEO  
**Subject:** Administrative Award of Contracts (Revenues) Pursuant To Resolution No. 557 for the Month of June 2023

---

## BACKGROUND

At the July 14, 2022 meeting of the Board of Trustees of the Reno-Tahoe Airport Authority, the Board approved Resolution No. 557, recognizing the inherent authority of the President/CEO, or authorized representative to award revenue contracts except those that are, in combination, more than 5 years in Term, including options, and generate aggregate revenues of \$250,000 or more.

## DISCUSSION

Resolution No. 557 requires that the President/CEO provide the Board of Trustees with an administrative report setting forth a list of revenue contracts and associated options to extend approved administratively as a result of the resolution to be given to the Board on a monthly basis.

### June 2023

Date	Lessee	Property Address	Agreement Term	Contract Value	Portfolio
6/1/23	Service Master by Dixon, LLC	2900 Vassar St. Suite CC-11	12 Months	\$6,531.60	Outside Properties
6/1/23	Service Master by Dixon, LLC	2900 Vassar St. Suite CC-17	12 Months	\$6241.20	Outside Properties
6/1/23	William Swope	2890 Vassar St, Suite AA-09	12 Months	\$7528.32	Outside Properties
6/1/23	Christopher McClintock	2890 Vassar St. Suite AA-10	12 Months	\$6578.28	Outside Properties
6/1/23	1 New Tenant	Mini Warehouse Storage	Month to Month	\$150.00	Outside Properties
6/1/23	4 New Tenants	Mini Warehouse Park to Travel	Month to Month	\$410.00	Outside Properties

# Administrative Report

**Date:** July 11, 2023  
**To:** All Board Members  
**From:** Daren Griffin, President/CEO  
**Subject:** Administrative Award of Contracts (Expenditures) Pursuant to Resolution No. 557 for the Month of June 2023

---

## BACKGROUND

At the July 14, 2022, Board of Trustees' Meeting of the Reno-Tahoe Airport Authority, the Board approved Resolution No. 557 authorizing the President/CEO to administratively award contracts for:

- budgeted professional services when the scope of work is \$200,000 or less, and to approve amendments where the sum of the total net of amendments per agreement does not to exceed \$50,000; and
- budgeted goods, materials, supplies, equipment, technical services, and maintenance contracts when the estimated amount to perform the contract, including all change orders, is \$250,000 or less; and
- budgeted construction contracts when the estimated amount to perform the work is \$500,000 or less, and approve change orders to construction contracts where the sum of the total net of change orders per contract does not exceed \$250,000; and
- budgeted Construction Management and Administration professional service agreements and amendments ("Work Order") where a single Work Order does not exceed \$250,000.

All construction contracts exceeding \$500,000 must be approved by the Board of Trustees, along with a request to establish an Owner's Contingency. Additionally, if the Board of Trustees originally approved the construction contract, any construction change order exceeding the sum of the total of the contract and Owner's Contingency must also be approved by the Board.

## DISCUSSION

Resolution No. 557 requires that the President/CEO provide the Board of Trustees with a monthly administrative report listing of all agreements and purchase orders in excess of \$25,000 and approved administratively as a result of this Resolution. Further, all change orders and amendments approved administratively as a result of this Resolution shall be included in a separate administrative report regardless of value.



### June 2023 – Agreements and POs in Excess of \$25,000

Date	Name of Company	Dollar Amount	Description	Funding Source	Department / Division
06/01/23	EYEP-Solutions Inc	\$48,214.29	A PO was issued for Extreme Networks Service Units.	FY23 O&M	Technology & Information Systems
06/15/23	Maise Group LLC	\$184,800.96	A PO was issued for Microsoft 365 GCC for 12 months.	FY23 O&M	Technology & Information Systems
06/15/23	Economic Development Authority Of	\$25,000.00	A PO was issued for semi-annual dues.	FY23 O&M	President/CEO
06/28/23	Maise Group LLC	\$43,194.78	A PO was issued for Annual Server Support.	FY23 O&M	Technology & Information Systems

### June 2023 – Change Orders, Amendments, and Work Orders

Date	Name of Company	Dollar Amount	Description	Funding Source	Department / Division
12/14/22	Tungsten Engineering	(\$6,000.00)	CO#2 for the RTS Erosion Control Project was final adjusted quantities (see attached).	FY23 CIP	Engineering & Construction
05/16/23	PK Electrical, Inc.	\$25,000.00	Amendment #1 for the Electrical Masterplan Equipment Evaluation added additional scope to include the terminal and Maintenance Buildings at RTS, as well as the Fire House, SRE, Airfield Maintenance, and AVA Buildings at RNO. Revised PSA, including Amendment #1, equals \$75,000.00.	FY23 O&M	Engineering & Construction
05/18/23	Mead & Hunt, Inc.	\$0	A \$0 amendment was applied to the base contract for the Concourse Planning Study to add scope for consulting services related to FAA AIP and BIL funding for the CUP. Although the base contract was a fixed fee, the consultant determined approximately \$39,909 of remaining budget towards the new scope.	FY19 CIP	Planning & Environmental
05/25/23	Granite Construction Co.	(\$1,646,954.45)	Deductive CO#6 applied base bid and bid alt #2 final adjusted quantities (see attached). Final contract sum, including this CO, equals \$41,724,221.38.	Multiple Funding Sources / Fiscal Years: CIP and AIP Grants	Engineering & Construction
06/05/23	Reyman Brothers Construction, Inc.	\$48,808.64	Owner's Contingency CO#3 for the Airfield Admin and Airport Ops Admin Offices Remodel Project for additional scope of work (see attached). Revised contract sum, including OCCO#3, equals \$1,655,676.38.	FY23 CIP	Engineering & Construction
06/06/23	Gardner Engineering	\$5,886.00	Owner's Contingency CO#2 for the MZ-3 Replacement Project provides for additional scope of work (see attached). Revised contract sum, including OCCO#2, equals \$312,405.00.	FY23 CIP	Engineering & Construction
06/22/23	Gardner Engineering	\$3,533.00	Owner's Contingency CO#3 for the MZ-3 Replacement Project provides for additional scope of work (see attached). Revised contract sum, including OCCO#3, equals \$315,938.00.	FY23 CIP	Engineering & Construction
06/27/23	McCarthy Building Companies, Inc.	(\$10,000.00)	CO#3, a deductive CO, for decreasing Schedule of Values in Owner's Contingency (Ticketing Hall Expansion Project, GMP #2) (see attached). Revised GMP, including this CO, equals \$25,881,971.68.	FY23 CIP	Engineering & Construction

**Key to abbreviations:**

AIP = Airport Improvement Project  
 CIP = Capital Improvement Program  
 CFC = Customer Facility Charge

CO = Change Order  
 NTE = Not to Exceed  
 PFC = Passenger Facility Charge

WO = Construction Mgmt Work Order  
 PO = Purchase Order  
 PSA = Professional Service Agreement

**CHANGE  
ORDER****Distribution to:**

RTAA PURCHASING	<input checked="" type="checkbox"/>
PM	<input checked="" type="checkbox"/>
CM	<input checked="" type="checkbox"/>
ENGINEER	<input checked="" type="checkbox"/>
CONTRACTOR	<input checked="" type="checkbox"/>
FAA	<input type="checkbox"/>

**Reno-Tahoe Airport Authority**

Reno-Tahoe International Airport  
Reno-Stead Airport  
Box 12490  
Reno, NV 89510



Project: Reno-Stead Erosion Control Project

Solicitation #: ITB #22/23-11

To: Tungsten Engineering Contractors, LLC  
5470 Kietzke Lane, Suite 337  
Reno, NV 89511

Change Order Number 02

Change Order Initiation Date: December 12, 2022

AIP No. N/A

Original Contract Date: October 3, 2022

**You are directed to make the following changes in the Contract:**

**Final Adjusted Quantities (see exhibit A) ..... (\$6,000.00)**

**All other terms, conditions, and requirements not modified herein remain unchanged.**

Not valid until signed by ALL parties. Execution of this Change Order by both Owner and Contractor constitutes a binding agreement and serves as a full accord and satisfaction of any claim, demand, lien, stop notice or further request for compensation, past or present, known or unknown, and/or time extension arising out of or by virtue of the work described above in the Change Order. Contractor's signature indicates agreement herewith, including any adjustments in the Contract Sum or Contract Time.

The Original Contract Sum was.....	\$320,074.00
Net Changes by Previously Authorized Change Orders .....	\$59,230.00
Net Changes by Previously Authorized Contingency Change Orders .....	\$0.00
The Revised Contract Sum Prior to this Change Order was .....	\$379,304.00
The Contract Sum will be <b>decreased</b> by this Change Order.	(\$6,000.00)
The new Contract Sum, including this Change Order will be .....	\$373,304.00

The Contract Completion date prior to this Change Order was November 30, 2022.

The Contract Time will not change due to this change order.

Authorized By:

**Atkins North America**

Construction Manager  
10509 Professional Cir. Ste 103  
Reno, NV 89521

By: Brian Janes

12/12/22

Date

**Atkins North America**

Construction Manager  
10509 Professional Cir. Ste 103  
Reno, NV 89521

By: Brian Janes

12/12/22

Date

**Tungsten Engineering**

Contractor  
5470 Kietzke Lane, Suite 337  
Reno, NV 89511

By: Heather Hellickson

12/14/22

Date

**Reno-Tahoe Airport Authority**

Owner  
P.O. Box 12490  
Reno, NV 89510

By: Chris Cobb

12/14/2022

Date

# Exhibit A

# RTS EROSION CONTROL PROJECT - FINAL ADJUSTED QUANTITIES

Item #	Description		Quantity				Cost			
		Unit	Estimated	Measured	Over/Under	% Complete	Unit Cost	Bid Price	\$ Extended (Measured)	\$ Over/Under
1	Airport Safety & Security	LS	1	1.00	0.00	100.00%	\$1,500.00	\$ 1,500.00	\$ 1,500.00	\$ -
2	Clearing and Grubbing	LS	1	1.00	0.00	100.00%	\$10,000.00	\$ 10,000.00	\$ 10,000.00	\$ -
3	Remove and Replace Perimeter Fence (Contingent)	LF	30	-	(30.00)	0.00%	\$200.00	\$ 6,000.00	\$ -	\$ (6,000.00)
4	Earthwork	CY	493	493.00	0.00	100.00%	\$60.00	\$ 29,580.00	\$ 29,580.00	\$ -
5	Install 12" D50 Riprap with Non-Woven Geotextile	CY	74	74.00	0.00	100.00%	\$400.00	\$ 29,600.00	\$ 29,600.00	\$ -
6	Install 24" D50 Riprap with Non-Woven Geotextile	CY	278	278	0.00	100.00%	\$250.00	\$ 69,500.00	\$ 69,500.00	\$ -
7	Install 3'x3'x3' Gabion Baskets with 6" Rock and Non-Woven Geotextile	EA	99	99	0.00	100.00%	\$900.00	\$ 89,100.00	\$ 89,100.00	\$ -
8	Install 12" Thick Gabion Mattress with Non-Woven Geotextile	SF	1,305	1,305	0.00	100.00%	\$35.00	\$ 45,675.00	\$ 45,675.00	\$ -
9	Place Type 2, Class B Aggregate Base for Maintenance Road (Contingent)	CY	64	64.00	0.00	100.00%	\$250.00	\$ 16,000.00	\$ 16,000.00	\$ -
10	Mobilization	LS	1	1.00	0.00	100.00%	\$23,119.00	\$ 23,119.00	\$ 23,119.00	\$ -
	CHANGE ORDERS							\$ -	\$ -	\$ -
1	Install D50 Rip Rap and Additional excavation for channel realignment	LS	1	1.00	0.00	100.00%	\$ 59,230.00	\$ 59,230.00	\$ 59,230.00	\$ -
	Total							\$ 379,304.00	\$ 373,304.00	\$ (6,000.00)

**CHANGE  
ORDER****Distribution to:**

RTAA PURCHASING	<input checked="" type="checkbox"/>
PM	<input checked="" type="checkbox"/>
CM	<input checked="" type="checkbox"/>
ENGINEER	<input checked="" type="checkbox"/>
CONTRACTOR	<input checked="" type="checkbox"/>
FAA	<input checked="" type="checkbox"/>

**Reno-Tahoe Airport Authority**

Reno-Tahoe International Airport  
Reno-Stead Airport  
P.O. Box 12490  
Reno, NV 89510



Project: Runway 16R-34L Reconstruction Project  
RTAA Project #: R19005A

Change Order Number 06  
Initiation Date: May 11, 2023  
AIP No. 3-32-0017-106  
Original Contract Date: April 8, 2020

To: Granite Construction Company  
P.O. Box 2087  
Sparks, NV 89431

**You are directed to make the following changes in the Contract:**

PCO No. 08 – MagVar Striping & Signage Changes	(\$15.68)
PCO No. 23 – East Airfield Lighting Vault Fiber Connectivity	(\$1.32)
PCO No. 26 – Spare Runway Light Fixtures	(\$2.94)
PCO No. 28 – Remove and Replace Electrical Vault Lids	(\$0.18)
PCO No. 29 – Additional Spare Runway Light Fixtures	(\$0.07)
PCO No. 30 – Procure Additional Paint and Beads	(\$1.75)
Base Bid Final Adjusted Quantities	17,073.06
Bid Alternate No. 02 Final Adjusted Quantities	<u>(1,664,005.57)</u>

**Total (\$1,646,954.45)**

**All other terms, conditions, and requirements not modified herein remain unchanged.**

Not valid until signed by ALL parties. Execution of this Change Order by both Owner and Contractor constitutes a binding agreement and serves as a full accord and satisfaction of any claim, demand, lien, stop notice or further request for compensation, past or present, known or unknown, and/or time extension arising out of or by virtue of the work described above in the Change Order. Contractor's signature indicates agreement herewith, including any adjustments in the Contract Sum or Contract Time.

The Original Contract Sum was.....	\$44,518,615.00
Net Changes by Previously Authorized Change Orders .....	(\$1,147,439.17)
Net Changes by Previously Authorized Contingency Change Orders .....	\$0.00
The Revised Contract Sum Prior to this Change Order was .....	\$43,371,175.83
The Contract Sum will be <b>decreased</b> by this Change Order.	(\$1,646,954.45)
The new Contract Sum, including this Change Order will be .....	\$41,724,221.38

The Contract Completion date prior to this Change Order was October 8, 2021.

The Contract time was suspended from October 17, 2021, through October 9, 2022

The Contract Time will be increased by fifty-three (53) calendar days until MagVar work commenced on October 10, 2022.

The Contract completion date, as of the date of this Change Order, therefore is November 23, 2022.

Authorized By:

**Atkins North America**  
Construction Manager  
10509 Professional Cir. Ste 103  
Reno, NV 89521

By: Kara Bymers

05/11/2023

Date

**Wood Rodgers, Inc.**  
Engineer  
1361 Corporate Blvd  
Reno, NV 89502

Digitally signed by Brian  
Martinezmoles

Date: 2023.05.22 16:05:23 -07'00'

By: Brian Martinezmoles

Date

**Granite Construction Co.**  
Contractor  
P.O. Box 2087  
Sparks, NV 89431

By: Matt Cates

5/15/23

Date

**Reno-Tahoe Airport Authority**  
Owner  
P.O. Box 12490  
Reno, NV 89510

By:

5/25/2023

Date

Enclosure: Exhibit A – PCO Coversheets  
Exhibit B – Base Bid Final Adjusted Quantities  
Exhibit C – Bid Alternate No. 02 Final Adjusted Quantities

# Exhibit A



Member of the SNC-Lavalin Group

Atkins North America, Inc.  
10509 Professional Circle, Suite 102  
Reno, NV 89521-8991  
**Telephone: +1.775.828.1622**  
Fax: +1.775.851.1687  
[www.atkinsglobal.com/northamerica](http://www.atkinsglobal.com/northamerica)

March 30, 2023

Mr. Matt Cates  
Area Manager  
Granite Construction Company  
1900 Glendale Avenue  
Sparks, NV 89431

**Subject:** Runway 16R-34L Reconstruction Project  
Reno-Tahoe International Airport  
**Potential Change Order No. 08**  
**MagVar Striping & Signage Changes**

Dear Mr. Cates:

This Potential Change Order (PCO) sets forth the terms and conditions to provide labor, equipment, material and full compensation to perform the required MagVar changes that were rescheduled to begin October 2022 due to FAA COVID-19 shutdown. The cost for this work is offset with deleting unused contingent items and reducing unused quantity of Bid Item L-108-1.

**The following adjustment shall be made to Bid Alternate 2 Schedule of Values:**

PCO 08-01, MagVar Striping & Signage Changes	
• One Lump Sum price	\$256,806.12
Bid Item P-152-3, Excavation & Embankment of Unsuitable Material (Contingent)	
• Delete the remaining quantity of 2,945 CY (2,954 CY @ \$42.00/CY)	(\$124,068.00)
Bid Item L-108-1, 1-1/C No. 8 AWG, 5kV, L-824C Cable	
• Delete the remaining quantity of 66,648 LF (66,648 LF @ \$1.60/LF)	(\$106,636.80)
Bid Item P-209-2, Crushed Aggregate Base Leveling Course (P-209) (Contingent)	
• Decrease the estimated quantity of 5,000 Ton to 3,726 Ton (1,274 Ton @ \$20.50/Ton)	(\$26,117.00)
<b>Total</b>	<b>(\$15.68)</b>

The total deductive amount approved for this change is **(\$15.68)**. Granite Construction Company will receive 50 calendar days added to the contract duration for this change.

If the above terms and conditions are acceptable, please indicate by signing below and returning the original letter to this office. Upon receiving all approvals, the necessary change order documentation will be processed. A fully executed copy of this PCO will be forwarded to Granite Construction Company. If you have any questions, please call me at (775) 828-1622.



Sincerely,



Kara Bymers  
Senior Construction Manager

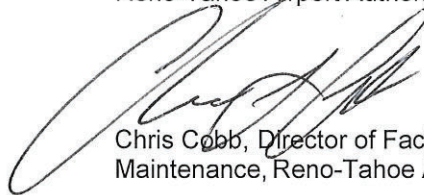


Matt Cates, Area Manager  
Granite Construction Company

Concur:



Tony Curatolo, Project Manager  
Reno-Tahoe Airport Authority



Chris Cobb, Director of Facilities and  
Maintenance, Reno-Tahoe Airport Authority

Enclosure: Granite Construction's Time and Material Sheets.

File: 40.08.08



Member of the SNC-Lavalin Group

Atkins North America, Inc.  
10509 Professional Circle, Suite 102  
Reno, NV 89521-8991  
**Telephone: +1.775.828.1622**  
Fax: +1.775.851.1687  
[www.atkinsglobal.com/northamerica](http://www.atkinsglobal.com/northamerica)

December 17, 2021

Mr. Matt Cates  
Area Manager  
Granite Construction Company  
1900 Glendale Avenue  
Sparks, NV 89431

**Subject:** Runway 16R-34L Reconstruction Project  
Reno-Tahoe International Airport  
**Potential Change Order No. 23**  
**East Airfield Lighting Vault Fiber Connectivity and Contingent Bid Item Deduct**  
Letter Serial No. R19005A-OGCC-271

Dear Mr. Cates:

This Potential Change Order (PCO) sets forth the terms and conditions to provide labor, equipment, material and full compensation to upgrade the existing fiber optic communication line between the West Airfield Lighting Vault to the East Airfield Lighting Vault. The cost of this work is offset with deleting unused quantity of contingent bid items.

**The following adjustments shall be made to Base Bid Schedule of Values:**

PCO 23-01, Single Mode Fiber Optic and Associated Hardware Installation	
• One lump sum	\$50,698.68
Bid Item P-152-3, Excavation and Embankment of Unsuitable Material (Contingent)	
• Delete the remaining quantity (600 CY @ \$42.00/CY)	(\$25,200.00)
Bid Item P-152-5, Disposal of Contaminated Material (Contingent)	
• Delete the remaining quantity (118 CY @ \$125.00/CY)	(\$14,750.00)
Bid Item P-620-6, Miscellaneous Permanent Reflective Airfield Pavement Markings (Contingent Item)	
• Decrease the estimated quantity of 50,000 SF to 39,250 SF (10,750 SF @ \$1.00/SF)	<u>(\$10,750.00)</u>
Total	(\$1.32)

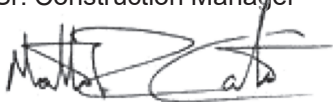
The total deductive amount approved for this change is **(\$1.32)**. Granite Construction Company has not asked for and will not receive any days added to the contract duration for this change.

If the above terms and conditions are acceptable, please indicate by signing below and returning the original letter to this office. Upon receiving all approvals, the necessary change order documentation will be processed. A fully executed copy of this PCO will be forwarded to Granite Construction Company. If you have any questions, please call me at (775) 828-1622.

Sincerely,



Joe Mamola  
Sr. Construction Manager

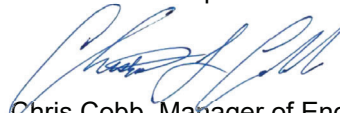


Matt Cates, Area Manager  
Granite Construction Company

Concur:



Tony Curatolo, Project Manager  
Reno-Tahoe Airport Authority



Chris Cobb, Manager of Engineering &  
Construction Reno-Tahoe Airport Authority

Enclosure: Granite Construction's Time and Material Sheets.

File: R19005A.70.03; R19005A.40.08.23



Member of the SNC-Lavalin Group

**Atkins North America, Inc.**  
10509 Professional Circle, Suite 102  
Reno, NV 89521-8991  
**Telephone: +1.775.828.1622**  
Fax: +1.775.851.1687  
**[www.atkinsglobal.com/northamerica](http://www.atkinsglobal.com/northamerica)**

December 17, 2021

Mr. Matt Cates  
Area Manager  
Granite Construction Company  
1900 Glendale Avenue  
Sparks, NV 89431

**Subject:** Runway 16R-34L Reconstruction Project  
Reno-Tahoe International Airport  
**Potential Change Order No. 26**  
**Spare Runway Light Fixtures**  
Letter Serial No. R19005A-OGCC-272

Dear Mr. Cates:

This Potential Change Order (PCO) sets forth the terms and conditions to purchase and deliver to the owner, the following spare runway light fixtures:

3 EA Runway Centerline Lights Red/White  
3 EA Runway Centerline Lights White/White  
3 EA Elevated Runway Edge Lights White/White  
3 EA Elevated Runway Edge Lights White/Yellow  
1 EA Runway End light – Left - Red/Red  
1 EA Runway End Light – Right - Red/Red  
1 EA Displaced Threshold – Left - Green/Blank  
1 EA Displaced Threshold – Right - Green/Blank

The cost of this material is offset with deleting unused quantity of contingent item P-620-6.

**The following adjustments shall be made to Base Bid Schedule of Values:**

PCO 26-01, Spare Runway Lights	
• One lump Sum	\$15,022.06
Bid Item P-620-6, Miscellaneous Permanent Reflective Airfield Pavement Markings (Contingent Item)	
• Decrease the estimated quantity of 39,250 SF to 24,225 SF (15,025 SF @ \$1.00/SF)	(\$15,025.00)

The total deductive amount approved for this change is **(\$2.94)**. Granite Construction Company has not asked for and will not receive any days added to the contract duration for this change.

If the above terms and conditions are acceptable, please indicate by signing below and returning the original letter to this office. Upon receiving all approvals, the necessary change order documentation will be processed. A fully executed copy of this PCO will be forwarded to Granite Construction Company. If you have any questions, please call me at (775) 828-1622.

Sincerely,



Joe Mamola  
Sr. Construction Manager



Matt Cates, Area Manager  
Granite Construction Company

Concur:



Tony Curatolo, Project Manager  
Reno-Tahoe Airport Authority



Chris Cobb, Manager of Engineering &  
Construction Reno-Tahoe Airport Authority

Enclosure: Granite Construction's Time and Material Sheets.

File: R19005A.70.03; R19005A.40.08.26



Member of the SNC-Lavalin Group

Atkins North America, Inc.  
10509 Professional Circle, Suite 103  
Reno, NV 89521-8991  
Telephone: +1.775.828.1622  
Fax: +1.775.851.1687  
[www.atkinsglobal.com/northamerica](http://www.atkinsglobal.com/northamerica)

March 30, 2023

Mr. Matt Cates  
Area Manager  
Granite Construction Company  
1900 Glendale Avenue  
Sparks, NV 89431

**Subject:** Runway 16R-34L Reconstruction Project  
Reno-Tahoe International Airport  
**Potential Change Order No. 28**  
**Remove and Replace Electrical Vault Lids**

Dear Mr. Cates:

This Potential Change Order (PCO) sets forth the terms and conditions to provide labor, equipment, material and full compensation to remove and replace six (6) 36-inch electrical vault lids. The cost for this work is offset with deleting unused quantity of Bid Item L-108-2.

**The following adjustment shall be made to Bid Alternate 2 Schedule of Values:**

PCO 28-01, Remove and Replace Electrical Vault Lids	
• One Lump Sum price	\$50,254.22
Bid Item L-108-2, 1-1/C No. 6 AWG, 600V, Ground Wire	
• Decrease the estimated quantity of 104,196 LF to 41,378 LF (62,818 LF @ \$0.80/LF)	(\$50,254.40)
<b>Total</b>	<b>(\$0.18)</b>

The total deductive amount approved for this change is **(\$0.18)**. Granite Construction Company has not asked for and will not receive any days added to the contract duration for this change.

If the above terms and conditions are acceptable, please indicate by signing below and returning the original letter to this office. Upon receiving all approvals, the necessary change order documentation will be processed. A fully executed copy of this PCO will be forwarded to Granite Construction Company. If you have any questions, please call me at (775) 828-1622.

Sincerely,

Kara Bymers  
Senior Construction Manager

Matt Cates, Area Manager  
Granite Construction Company

Concur:

Tony Curatolo, Project Manager  
Reno-Tahoe Airport Authority

Chris Cobb, Director of Facilities and  
Maintenance, Reno-Tahoe Airport Authority

Enclosure: Granite Construction's Time and Material Sheets.

File: 40.08.08





Member of the SNC-Lavalin Group

Atkins North America, Inc.  
10509 Professional Circle, Suite 103  
Reno, NV 89521-8991  
Telephone: +1.775.828.1622  
Fax: +1.775.851.1687  
[www.atkinsglobal.com/northamerica](http://www.atkinsglobal.com/northamerica)

March 30, 2023

Mr. Matt Cates  
Area Manager  
Granite Construction Company  
1900 Glendale Avenue  
Sparks, NV 89431

**Subject:** Runway 16R-34L Reconstruction Project  
Reno-Tahoe International Airport  
**Potential Change Order No. 29**  
**Additional Spare Runway Light Fixtures**

Dear Mr. Cates:

This Potential Change Order (PCO) sets forth the terms and conditions to purchase and deliver to the owner, the following spare runway light fixtures:

2 EA, L-850C BI,W-L,W-R,60Hz,NO M,1CD,W/AK  
1 EA, L-850C BI,Y-L,W-R,60Hz,NO M,1CD,W/ AK

The cost of this material is offset with deleting unused quantity of Bid Item P-620-5.

**The following adjustments shall be made to Base Bid Schedule of Values:**

PCO 29-01, Spare Runway Lights

- One lump Sum \$4,480.93

Bid Item P-620-5, Permanent Reflective Airfield Pavement Markings

- Decrease the estimated quantity of 410,000 SF to 405,519 SF  
(4,481 SF @ \$1.00/SF) (\$4,481.00)

The total deductive amount approved for this change is **(\$0.07)**. Granite Construction Company has not asked for and will not receive any days added to the contract duration for this change.

If the above terms and conditions are acceptable, please indicate by signing below and returning the original letter to this office. Upon receiving all approvals, the necessary change order documentation will be processed. A fully executed copy of this PCO will be forwarded to Granite Construction Company. If you have any questions, please call me at (775) 828-1622.

Sincerely,

Kara Bymers  
Senior Construction Manager

Matt Cates, Area Manager  
Granite Construction Company

Concur:

Tony Curatolo, Project Manager  
Reno-Tahoe Airport Authority

Chris Cobb, Director of Facilities and  
Maintenance, Reno-Tahoe Airport Authority

Enclosure: Granite Construction's Time and Material Sheets

File: 40.08.29



Member of the SNC-Lavalin Group

Atkins North America, Inc.  
10509 Professional Circle, Suite 102  
Reno, NV 89521-8991  
**Telephone: +1.775.828.1622**  
Fax: +1.775.851.1687  
[www.atkinsglobal.com/northamerica](http://www.atkinsglobal.com/northamerica)

March 30, 2023

Mr. Matt Cates  
Area Manager  
Granite Construction Company  
1900 Glendale Avenue  
Sparks, NV 89431

**Subject:** Runway 16R-34L Reconstruction Project  
Reno-Tahoe International Airport  
**Potential Change Order No. 30**  
**Procure Additional Paint and Beads**

Dear Mr. Cates:

This Potential Change Order (PCO) sets forth the terms and conditions to purchase and deliver to the owner deliver three (3) totes of Ozark white paint and 6,000 pounds of Swarco Type 3 glass beads. The cost of this material is offset with deleting unused quantity of contingent item P-620-3.

**The following adjustment shall be made to Base Bid Schedule of Values:**

PCO 30-01, Furnish White Striping Paint & Glass Beads

- One Lump Sum price \$48,871.35

Bid Item P-620-3, Thermoplastic Runway Holding Position Surface Painted Sign Removal

- Decrease the estimated quantity of 25,000 SF to 20,694 SF  
(4306 SF @ \$11.35/SF) (\$48,873.10)

**Total (\$1.75)**

The total deductive amount approved for this change is **(\$1.75)**. Granite Construction Company has not asked for and will not receive any days added to the contract duration for this change.

If the above terms and conditions are acceptable, please indicate by signing below and returning the original letter to this office. Upon receiving all approvals, the necessary change order documentation will be processed. A fully executed copy of this PCO will be forwarded to Granite Construction Company. If you have any questions, please call me at (775) 828-1622.

Sincerely,

Kara Bymers  
Senior Construction Manager

Matt Cates, Area Manager  
Granite Construction Company

Concur:

Tony Curatolo, Project Manager  
Reno-Tahoe Airport Authority

Chris Cobb, Director of Facilities and  
Maintenance, Reno-Tahoe Airport Authority

Enclosure: Granite Construction's Time and Material Sheets.

File: 40.08.08



# Exhibit B

## Runway 16R-34L Project Phase 2

Base Bid Final Adjusted Quantities										
Item #	Description	Unit	Quantity				Cost			
			Estimated	Measured	Over/Under	% Complete	Unit Cost	Bid Price	\$ Extended (Measured)	\$ Over/Under
C-105-1	Mobilization	LS	1	1.00	0.00	100.00%	\$ 755,000.00	\$ 755,000.00	\$ 755,000.00	\$ -
C-106-1	Airport Safety and Security	LS	1	1.00	0.00	100.00%	\$ 2,321,078.50	\$ 2,321,078.50	\$ 2,321,078.50	\$ -
P-101-1	Remove Bituminous Pavement Section (2-6 inch thick)	SY	31,750	31,820.80	70.80	100.22%	\$ 5.50	\$ 174,625.00	\$ 175,014.40	\$ 389.40
P-101-2	Remove Pavement Section (15-20 PCCP & 6 to 8 inch ATPB/CTB) (Modified by PCO 16)	SY	2,139	1,936.00	(203.00)	90.51%	\$ 76.00	\$ 162,564.00	\$ 147,136.00	\$ (15,428.00)
P-152-1	Unclassified Excavation and Export	CY	3,500	3,500.00	0.00	100.00%	\$ 20.00	\$ 70,000.00	\$ 70,000.00	\$ -
P-152-2	Unclassified Excavation and Embankment	CY	2,000	2,000.00	0.00	100.00%	\$ 11.00	\$ 22,000.00	\$ 22,000.00	\$ -
P-152-3	Excavation and Embankment of Unsuitable Material (Contingent) (Modified By PCO 14, 23)	CY	0	-	0.00	100.00%	\$ 42.00	\$ -	\$ -	\$ -
P-152-4	Subgrade/Subbase Preparation (Modified by PCO 16)	SY	11,114	12,501.00	1387.00	112.48%	\$ 2.50	\$ 27,785.00	\$ 31,252.50	\$ 3,467.50
P-152-5	Disposal of Contaminated Material (Contingent) (Modified By PCO 14, 19, 23)	CY	0	-	0.00	100.00%	\$ 125.00	\$ -	\$ -	\$ -
P-201S-1	Asphalt Treated Permeable Base (P-201) (6 inch thick) (Modified by PCO 16)	SY	1,147	1,095.50	(51.50)	95.51%	\$ 20.00	\$ 22,940.00	\$ 21,910.00	\$ (1,030.00)
P-209-1	Crushed Aggregate Base Course (P-209) (6 inch thick)	SY	9,000	10,565.00	1565.00	117.39%	\$ 11.00	\$ 99,000.00	\$ 116,215.00	\$ 17,215.00
P-403-1	Hot Mix Asphalt (HMA) Pavement (P-403) (6 inch thick)	SY	9,000	10,565.00	1565.00	117.39%	\$ 32.00	\$ 288,000.00	\$ 338,080.00	\$ 50,080.00
P-403-2	Hot Mix Asphalt (HMA) Base (P-403) (6 inch thick) (Modified by PCO 16)	SY	967	840.50	(126.50)	86.92%	\$ 32.00	\$ 30,944.00	\$ 26,896.00	\$ (4,048.00)
P-501-1	Portland Cement Concrete Pavement (P-501) (17 inch thick) (Modified by PCO 16)	SY	2,114	1,862.21	(251.79)	88.09%	\$ 164.00	\$ 346,696.00	\$ 305,402.44	\$ (41,293.56)
P-620-1	Airfield Striping Removals	SF	375,000	457,859.76	82859.76	122.10%	\$ 1.15	\$ 431,250.00	\$ 526,538.72	\$ 95,288.72
P-620-2	Airfield Striping Removals (Contingent Item) (Modified By PCO 14)	SF	125,000	6,744.00	(118256.00)	5.40%	\$ 0.60	\$ 75,000.00	\$ 4,046.40	\$ (70,953.60)
P-620-3	Thermoplastic Runway Holding Position Surface Painted Sign Removal (Modified By PCO 30)	SF	20,694	19,725.25	(968.75)	95.32%	\$ 11.35	\$ 234,876.90	\$ 223,881.60	\$ (10,995.30)
P-620-4	Airfield Striping Cleaning	SF	125,000	161,756.00	36756.00	129.40%	\$ 0.45	\$ 56,250.00	\$ 72,790.20	\$ 16,540.20
P-620-5	Permanent Reflective Airfield Pavement Markings (Modified By PCO 29)	SF	405,519	358,951.00	(46568.00)	88.52%	\$ 1.00	\$ 405,519.00	\$ 358,951.00	\$ (46,568.00)
P-620-6	Miscellaneous Permanent Reflective Airfield Pavement Markings (Contingent Item) (Modified By PCO 14, 26)	SF	24,225	11,119.25	(13105.75)	45.90%	\$ 1.00	\$ 24,225.00	\$ 11,119.25	\$ (13,105.75)
P-620-7	Permanent Non-Reflective Airfield Pavement Marking	SF	155,000	156,223.75	1223.75	100.79%	\$ 0.60	\$ 93,000.00	\$ 93,734.25	\$ 734.25
P-620-8	Miscellaneous Permanent Non-Reflective Airfield Pavement Marking (Contingent Item) (Modified By PCO 14)	SF	20,000	7,385.00	(12615.00)	36.93%	\$ 0.60	\$ 12,000.00	\$ 4,431.00	\$ (7,569.00)
P-620-9	Thermoplastic Runway Holding Position Surface Painted Sign	EA	38	41.00	3.00	107.89%	\$ 12,000.00	\$ 456,000.00	\$ 492,000.00	\$ 36,000.00
P-620-10	Thermoplastic Taxiway Directional Marking	EA	3	3.00	0.00	100.00%	\$ 12,000.00	\$ 36,000.00	\$ 36,000.00	\$ -
P-621-5.1	Grooving (Modified by PCO 16)	SY	2,114	1,935.00	(179.00)	91.53%	\$ 34.00	\$ 71,876.00	\$ 65,790.00	\$ (6,086.00)
T-908-1	Remove, Stockpile, and Reset Existing Surface Rock (3-6 inch thick)	SY	30,500	13,848.70	(16651.30)	45.41%	\$ 1.00	\$ 30,500.00	\$ 13,848.70	\$ (16,651.30)
T-908-2	Import and Place Surface Rock (3 inch thick)	SY	21,500	40,465.00	18965.00	188.21%	\$ 2.50	\$ 53,750.00	\$ 101,162.50	\$ 47,412.50
L-108-1	1-1/C No. 8 AWG, 5 kV, L-824C Cable (Modified By PCO 10a)	LF	1,216	1,216.00	0.00	100.00%	\$ 2.20	\$ 2,675.20	\$ 2,675.20	\$ -
L-108-2	1-1/C No. 6 AWG, 600V Ground Wire (Modified By PCO 10a)	LF	616	616.00	0.00	100.00%	\$ 1.40	\$ 862.40	\$ 862.40	\$ -
L-110-1	2W-3" FAA Ductbank	LF	785	576.00	(209.00)	73.38%	\$ 105.00	\$ 82,425.00	\$ 60,480.00	\$ (21,945.00)
L-110-2	2W-4" FAA Ductbank	LF	1,540	1,432.00	(108.00)	92.99%	\$ 107.00	\$ 164,780.00	\$ 153,224.00	\$ (11,556.00)
L-110-3	4W-3" FAA Ductbank	LF	2,150	2,321.00	171.00	107.95%	\$ 124.00	\$ 266,600.00	\$ 287,804.00	\$ 21,204.00
L-110-4	4W-4" FAA Ductbank	LF	260	177.00	(83.00)	68.08%	\$ 141.00	\$ 36,660.00	\$ 24,957.00	\$ (11,703.00)
L-110-5	1W-2" AFL Concrete Encased Electrical Conduit in New PCC	LF	210	-	(210.00)	0.00%	\$ 25.00	\$ 5,250.00	\$ -	\$ (5,250.00)
L-110-11	6W-2" FAA Ductbank	LF	310	358.00	48.00	115.48%	\$ 163.00	\$ 50,530.00	\$ 58,354.00	\$ 7,824.00
L-115-1	4' by 4' by 4' Electrical Handhole - Aircraft Rated (Modified By PCO 01)	EA	25	25.00	0.00	100.00%	\$ 24,000.00	\$ 600,000.00	\$ 600,000.00	\$ -
L-125-1	3-Mod Lighted Sign (L) on Existing Foundation	EA	5	5.00	0.00	100.00%	\$ 5,700.00	\$ 28,500.00	\$ 28,500.00	\$ -
L-125-2	Panel (Any Length) on Existing Sign (for Mag Var)	EA	57	64.00	7.00	112.28%	\$ 1,700.00	\$ 96,900.00	\$ 108,800.00	\$ 11,900.00
L-125-3	L-868B Base Can w/ Extension	EA	4	-	(4.00)	0.00%	\$ 1,700.00	\$ 6,800.00	\$ -	\$ (6,800.00)
L-128-1	Demolition and Removal of Electrical Work	LS	1	1.00	0.00	100.00%	\$ 194,000.00	\$ 194,000.00	\$ 194,000.00	\$ -
L-128-2	Temporary Airfield Electrical Work	LS	1	1.00	0.00	100.00%	\$ 61,000.00	\$ 61,000.00	\$ 61,000.00	\$ -
L-128-3	Miscellaneous Airfield Electrical Work	LS	1	1.00	0.00	100.00%	\$ 111,000.00	\$ 111,000.00	\$ 111,000.00	\$ -
L-146-1	RW 16R MALSR Threshold Bar	LS	1	1.00	0.00	100.00%	\$ 160,000.00	\$ 160,000.00	\$ 160,000.00	\$ -
L-146-2	RW 16R MALSR Station 2, 4, 6 & 8	LS	1	1.00	0.00	100.00%	\$ 192,000.00	\$ 192,000.00	\$ 192,000.00	\$ -
L-146-3	RW 16R MALSR Station 10	LS	1	1.00	0.00	100.00%	\$ 143,000.00	\$ 143,000.00	\$ 143,000.00	\$ -
L-146-4	RW 16R MALSR Station 12	LS	1	1.00	0.00	100.00%	\$ 46,000.00	\$ 46,000.00	\$ 46,000.00	\$ -
L-146-5	RW 16R MALSR Station 14	LS	1	1.00	0.00	100.00%	\$ 17,000.00	\$ 17,000.00	\$ 17,000.00	\$ -
L-146-6	RW 16R MALSR Station 16, 18 & 20	LS	1	1.00	0.00	100.00%	\$ 52,000.00	\$ 52,000.00	\$ 52,000.00	\$ -
L-146-7	RW 16R MALSR Station 22	LS	1	1.00	0.00	100.00%	\$ 17,000.00	\$ 17,000.00	\$ 17,000.00	\$ -
L-146-8	RW 16R MALSR Station 24	LS	1	1.00	0.00	100.00%	\$ 17,000.00	\$ 17,000.00	\$ 17,000.00	\$ -
L-146-9	RW 34L MALSR Threshold Bar	LS	1	1.00	0.00	100.00%	\$ 113,000.00	\$ 113,000.00	\$ 113,000.00	\$ -

Base Bid Final Adjusted Quantities										
Item #	Description	Unit	Quantity				Cost			
			Estimated	Measured	Over/Under	% Complete	Unit Cost	Bid Price	\$ Extended (Measured)	\$ Over/Under
L-146-10	RW 34L MALSR Station 2, 4, 6, 8, 10 & 12	LS	1	1.00	0.00	100.00%	\$ 362,000.00	\$ 362,000.00	\$ 362,000.00	\$ -
L-146-11	RW 34L MALSR Station 14	LS	1	1.00	0.00	100.00%	\$ 37,000.00	\$ 37,000.00	\$ 37,000.00	\$ -
L-146-12	RW 34L MALSR Station 16, 18 & 20	LS	1	1.00	0.00	100.00%	\$ 52,000.00	\$ 52,000.00	\$ 52,000.00	\$ -
L-146-13	RW 34L MALSR Station 22 & 24	LS	1	1.00	0.00	100.00%	\$ 36,000.00	\$ 36,000.00	\$ 36,000.00	\$ -
L-146-14	MALSR 16R Wiring, DU Cabinet, & Associated Connections	LS	1	1.00	0.00	100.00%	\$ 172,000.00	\$ 172,000.00	\$ 172,000.00	\$ -
L-146-15	MALSR 34L Wiring, DU Cabinet & Associated Connections	LS	1	1.00	0.00	100.00%	\$ 166,000.00	\$ 166,000.00	\$ 166,000.00	\$ -
L-146-16	MALSR Miscellaneous Work (Both Ends)	LS	1	1.00	0.00	100.00%	\$ 57,000.00	\$ 57,000.00	\$ 57,000.00	\$ -
13410A	ALCS Modifications	LS	1	1.00	0.00	100.00%	\$ 114,238.00	\$ 114,238.00	\$ 114,238.00	\$ -
PCO 01-01	Remove Existing Handholes and Add New Steel Tags	LS	1	1.00	0.00	100.00%	\$ 12,756.00	\$ 12,756.00	\$ 12,756.00	\$ -
PCO 02-01	Surface Thermoplastic Runway Holding Position Signs	LS	1	1.00	0.00	100.00%	\$ 28,654.75	\$ 28,654.75	\$ 28,654.75	\$ -
PCO 07-01	Abandon FAA Vaults	LS	1	1.00	0.00	100.00%	\$ 3,389.75	\$ 3,389.75	\$ 3,389.75	\$ -
PCO 13-01	Relocate Stored Thermoplastic	LS	1	1.00	0.00	100.00%	\$ 1,275.16	\$ 1,275.16	\$ 1,275.16	\$ -
PCO 16-01	Remove and Replace Existing Panels	LS	1	1.00	0.00	100.00%	\$ 153,446.00	\$ 153,446.00	\$ 153,446.00	\$ -
PCO 19-01	Additional MALSR Lights	LS	1	1.00	0.00	100.00%	\$ 11,752.73	\$ 11,752.73	\$ 11,752.73	\$ -
PCO 24-02	Thermoplastic at Taxiway Juliet	LS	1	1.00	0.00	100.00%	\$ 35,148.04	\$ 35,148.04	\$ 35,148.04	\$ -
PCO 29-01	Additional Spare Runway Light Fixtures	LS	1	1.00	0.00	100.00%	\$ 4,480.93	\$ 4,480.93	\$ 4,480.93	\$ -
PCO 30-01	Furnish White Striping Paint & Glass Beads	LS	1	1.00	0.00	100.00%	\$ 48,871.35	\$ 48,871.35	\$ 48,871.35	\$ -
PCO 26-01	Spare Runway Light Fixtures	LS	1	1.00	0.00	100.00%	\$ 15,022.06	\$ 15,022.06	\$ 15,022.06	\$ -
PCO 23-01	East Airfield Lighting Vault Fiber Connectivity	LS	1	1.00	0.00	100.00%	\$ 50,698.68	\$ 50,698.68	\$ 50,698.68	\$ -
	Total							\$ 10,127,595.45	\$ 10,144,668.51	\$ 17,073.06

# Exhibit C

## Runway 16R-34L Project Phase 2

Bid Alternate No. 02 Final Adjusted Quantities											
Item #	Description	Unit	Quantity				Cost				
			Estimated	Measured	Over/Under	% Complete	Unit Cost	Bid Price	\$ Extended (Measured)	\$ Over/Under	
C-105-1	Mobilization	LS	1	1.00	0.00	100.00%	\$ 1,385,000.00	\$ 1,385,000.00	\$ 1,385,000.00	\$ -	
C-105-2	Demobilization & Remobilization for Winter Shutdown (Contingent) (Modified By PCO 14)	LS	0	0.00	0.00	100.00%	\$ 5,000.00	\$ -	\$ -	\$ -	
C-106-1	Airport Safety & Security	LS	1	1.00	0.00	100.00%	\$ 2,168,946.00	\$ 2,168,946.00	\$ 2,168,946.00	\$ -	
P-101-2	Remove Pavement Section (15' - 20" PCCP & 6'-8" ATPB/CTB)	SY	76,750	77,507.80	757.80	100.99%	\$ 25.00	\$ 1,918,750.00	\$ 1,937,695.00	\$ 18,945.00	
P-152-1	Unclassified Excavation & Export	CY	500	-	(500.00)	0.00%	\$ 20.00	\$ 10,000.00	\$ -	\$ (10,000.00)	
P-152-2	Unclassified Excavation & Embankment	CY	500	-	(500.00)	0.00%	\$ 11.00	\$ 5,500.00	\$ -	\$ (5,500.00)	
P-152-3	Excavation & Embankment of Unsuitable Material (Contingent) (Modified By PCO 8, 14, 18, 25)	CY	129	129.00	0.00	100.00%	\$ 42.00	\$ 5,418.00	\$ 5,418.00	\$ -	
P-152-4	Subgrade/Subbase Preparation	SY	82,450	77,507.80	(4942.20)	94.01%	\$ 2.00	\$ 164,900.00	\$ 155,015.60	\$ (9,884.40)	
P-201S01	Asphalt Treated Permeable Base (P-201) (6 inch thick)	SY	71,050	71,505.00	455.00	100.64%	\$ 20.00	\$ 1,421,000.00	\$ 1,430,100.00	\$ 9,100.00	
P-209-2	Crushed Aggregate Base Leveling Course (P-209) (Contingent) (Modified By PCO 8)	Ton	3,726	2,772.43	(953.57)	74.41%	\$ 20.50	\$ 76,383.00	\$ 56,834.83	\$ (19,548.17)	
P-403-2	Hot Mix Asphalt (HMA) Base (P-403) (6 inch thick)	SY	5,700	5,762.00	62.00	101.09%	\$ 32.00	\$ 182,400.00	\$ 184,384.00	\$ 1,984.00	
P-501-1	Portland Cement Concrete Pavement (P-501) (17 inch thick)	SY	76,750	63,482.98	(13,267.02)	82.71%	\$ 120.00	\$ 9,210,000.00	\$ 7,617,957.60	\$ (1,592,042.40)	
P-621-5.1	Grooving	SY	73,500	74,924.00	1424.00	0.00%	\$ 4.00	\$ 294,000.00	\$ 299,696.00	\$ 5,696.00	
T-908-4	Net Change to process, crush, & use onsite PCCP (Base Bid) (Contingent)	SY	0	-	0.00	100.00%	\$ -	\$ -	\$ -	\$ -	
T-908-5	Net Change to process, crush, & use onsite PCCP (Bid Alt #1) (Contingent)	SY	0	-	0.00	100.00%	\$ -	\$ -	\$ -	\$ -	
T-908-6	Net Change to process, crush, & use onsite PCCP (Bid Alt #7) (Contingent)	SY	0	-	0.00	100.00%	\$ -	\$ -	\$ -	\$ -	
L-108-1	1-1/C No. 8 AWG, 5KV, L-824C Cable (In Project Area) (Modified By PCO 8, 10a)	LF	43,198	43,198.00	0.00	100.00%	\$ 1.60	\$ 69,116.80	\$ 69,116.80	\$ -	
L-108-2	1-1/C No. 6 AWG, 600V, Ground Wire (Modified By PCO 10a, 28)	LF	41,378	21,861.00	(19517.00)	52.83%	\$ 0.80	\$ 33,102.40	\$ 17,488.80	\$ (15,613.60)	
L-110-5	1W-2" AFL Concrete Encased Electrical Conduit in New PCC	LF	14,090	11,804.00	(2286.00)	83.78%	\$ 17.00	\$ 239,530.00	\$ 200,668.00	\$ (38,862.00)	
L-125-3	L-868B Base Can w/ Extension	EA	271	271.00	0.00	100.00%	\$ 2,100.00	\$ 569,100.00	\$ 569,100.00	\$ -	
L-125-17	L-850A(L) Runway Centerline Light	EA	219	219.00	0.00	100.00%	\$ 1,700.00	\$ 372,300.00	\$ 372,300.00	\$ -	
L-125-18	L-852A(L) Taxiway Centerline Bidirectional Light (Modified By PCO 04)	EA	49	49.00	0.00	100.00%	\$ 1,700.00	\$ 83,300.00	\$ 83,300.00	\$ -	
L-125-19	L-852A(L) Taxiway Centerline Bidirectional Light	EA	3	3.00	0.00	100.00%	\$ 1,800.00	\$ 5,400.00	\$ 5,400.00	\$ -	
L-128-1	Demolition & Removal of Electrical Work	LS	1	1.00	0.00	100.00%	\$ 68,000.00	\$ 68,000.00	\$ 68,000.00	\$ -	
L-128-2	Temporary Airfield Electrical Work	LS	1	1.00	0.00	100.00%	\$ 43,000.00	\$ 43,000.00	\$ 43,000.00	\$ -	
L-128-3	Miscellaneous Airfield Electrical Work	LS	1	1.00	0.00	100.00%	\$ 102,000.00	\$ 102,000.00	\$ 102,000.00	\$ -	
L-128-4	Temporary Airfield Electrical Work for Winter Shutdown (Contingent) (Modified By PCO 14)	LS	0	0.00	0.00	100.00%	\$ 20,000.00	\$ -	\$ -	\$ -	
L-132-1	Install RPU Enclosure and Electronics Installation	LS	1	1.00	0.00	100.00%	\$ 35,000.00	\$ 35,000.00	\$ 35,000.00	\$ -	
L-132-2	Install Surface Scan Sensor Cable - Type V	LF	28,000	25,700.00	(2300.00)	91.79%	\$ 3.60	\$ 100,800.00	\$ 92,520.00	\$ (8,280.00)	
L-132-3	Install Pavement Surface Sensor System	LS	1	1.00	0.00	100.00%	\$ 60,000.00	\$ 60,000.00	\$ 60,000.00	\$ -	
L-132-4	Furnishing Surface Scan Sensor(s), Cable & RPU Equipment (By Manufacturer Costs)	LS	1	1.00	0.00	100.00%	\$ 452,000.00	\$ 452,000.00	\$ 452,000.00	\$ -	
L-125-22	L-852B(L) Taxiway Centerline Bidirectional Light (Modified By PCO 04)	EA	7	7.00	0.00	100.00%	\$ 1,700.00	\$ 11,900.00	\$ 11,900.00	\$ -	
PCO 02-02	Surface Thermoplastic Runway Centerline Markings	LS	1	1.00	0.00	100.00%	\$ 77,414.18	\$ 77,414.18	\$ 77,414.18	\$ -	
PCO 18-01	Additional PCCP Sawcutting at Taxiway Juliet	LS	1	1.00	0.00	100.00%	\$ 20,978.16	\$ 20,978.16	\$ 20,978.16	\$ -	
PCO 18-02	Replace Existing Electrical Vault Lid at Taxiway Papa	LS	1	1.00	0.00	100.00%	\$ 614.27	\$ 614.27	\$ 614.27	\$ -	
PCO 25-01	Remote and Replace PCCP	LS	1	1.00	0.00	100.00%	\$ 16,874.18	\$ 16,874.18	\$ 16,874.18	\$ -	
PCO 08-01	MAGVAR	LS	1	1.00	0.00	100.00%	\$ 256,806.12	\$ 256,806.12	\$ 256,806.12	\$ -	
PCO 28-01	Vault Lids	LS	1	1.00	0.00	100.00%	\$ 50,254.22	\$ 50,254.22	\$ 50,254.22	\$ -	

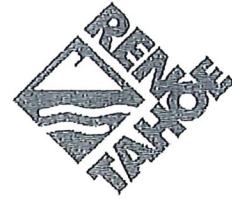
# RENO-TAHOE AIRPORT AUTHORITY OWNER'S CONTINGENCY CHANGE ORDER

CCO No.

Contractor:

Project:

Solicitation #:



## Summary of Change and List of Attachments:

Contingent Change Order No. 03  
Changes per attached Change Proposal Requests 6, 7, 8, and 10 (attached)

## Owner's Contingency Change Order Summary:

Owner's Contingency Total: \$

Total Previously Authorized: \$

Total Change this CCO: \$  ADD

Remaining Contingency Balance: \$

## Change in Contract Dates:

Original Duration (Days):

Previous Authorization:

This Authorization:  ADD

Revised Contract (Days):

## Contract Summary:

Original Contract: \$

Total Previously Authorized COs \$

Total Previously Authorized CCOs \$

Contract Sum Prior to this CCO \$

Total Change this Authorization: \$  ADD

New Contract Sum Incl this CCO: \$

## Distribution to:

RTAA PURCHASING X

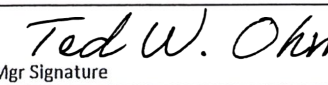
PM X

CM X

ENGINEER X

CONTRACTOR X

   
Contractor Signature Date:

   
Project Mgr Signature Date:

Contractor Name & Title:

RTAA Project Manager: Ted Ohm, Sr. Facilities Project Manager

N/A  
Construction Mgr Signature Date:

RTAA Mgr Signature Date:

RTAA Mgr Engineering & Construction: Jackie Borman

   
Engineer/Architect Signature Date:

Engineer/Architect Name & Title: Jeff Purcell, Project Manager

Not valid until signed by ALL parties. Execution of this Contingency Change Order by both Owner and Contractor constitutes a binding agreement and serves as a full accord and satisfaction of any claim, demand, lien, stop notice or further request for compensation, past or present, known or unknown, and/or time extension arising out of or by virtue of the work described above in the Contingency Change Order. Contractor's signature indicates agreement herewith, including any adjustments in the Contract Sum or Contract Time.



## CONSTRUCTION

"Committed to Your Success"

GENERAL BUILDING CONTRACTORS

6.1.23

### Change Proposal Request

6R

#### HOME OFFICE:

151 S. 18TH ST.  
SPARKS, NV 89431  
TEL: 775-356-0150  
FAX: 775-356-0247

Paul Cavin Architect, LLC  
1575 Delucchi Lane  
Reno NV 89502

re: RTAA Duty Manager Office Bldg. Remodel

Dear Paul,

reymanbrothers.com Presenting the following Change Proposal for review and approval.

Provide trench and backfill for fiberoptics between Airfiel Maitenance bldg and Airfield Shop Bldg. (See attached for initial trench proposal) Owner and General Contractor agree to split the cost of this trench work

#### LICENCES:

NEVADA  
#12481C

CALIFORNIA  
#702410

Total cost of trench \$53,411.52 divided by 2 (see attached costs)

\$26,705.75

sub-total  
OH&P @ 15%

TOTAL ADD \$26,705.75

CONTRACT TIME ADJUSTED:  
Add 15 Callendar Days

Sincerely,

Jace E. Callender  
Project Manager

Approved by: \_\_\_\_\_  
Paul Cavin Design

Date Approved: \_\_\_\_\_

## Darrell Linscott

---

**From:** Ohm, Ted (Facilities Project Manager) <toh@renoairport.com>  
**Sent:** Wednesday, May 31, 2023 10:03 AM  
**To:** Darrell Linscott; Jeff Purcell; paul@paulcavindesign.com  
**Subject:** Trenching

Darrell,

Just send over a revised Change Proposal Request indicating a 50% share, in the amount of \$26,705.76, for the RTAA, and I'll send the CO up the chain.

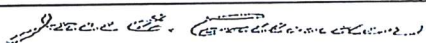
Ted W. Ohm  
Senior Project Manager  
Ext. 6423



wemoveyou  
Reno-Tahoe Airport Authority



## REQUEST FOR INFORMATION

PROJECT NAME: RTAA Duty Manager Office Bldg remodel		RESPOND BY:	ASAP
		GC'S RFI NO.	8
		SUB'S RFI NO.	
TO Paul Cavin Architects LLC		FROM: REYMAN BROS. CONSTRUCTION TEL: 775-356-0150 FAX: 775-356-0247	
RESPONSE BY:			
<input type="checkbox"/> System Eng. Consultants:		<input type="checkbox"/> Mechanical	
<input type="checkbox"/> Structural Eng. Consultants:		<input type="checkbox"/> Plumbing	
<input type="checkbox"/> XXX Other Consultants:		<input type="checkbox"/> Electrical	
SUBCONTRACTOR: RBC			
SUBJECT: trench for fiber			
CATEGORY:			
<input type="checkbox"/> XXX	NEED FOR CLARIFICATION	<input type="checkbox"/>	OMISSION
<input type="checkbox"/>	UNFORSEEN CONDITION	<input type="checkbox"/>	COORDINATION PROBLEM
<input type="checkbox"/>	CONFLICT WITHIN DOCUMENTS	<input type="checkbox"/>	OTHER
<u>SPEC. SECTION</u>	<u>PARAGRAPH NO.</u>	<u>DRAWING NO.</u>	<u>DETAIL NO.</u>
DESCRIPTION: On sheets TA 002 and TA101 there is reference to installation of a fiber optic cable. TA 002 shows a single line diagram (detail 8), a pull box detail (detail 7) , and a telecom duct bank detail (detail 6). None of these details provide enough information to show what exactly is to be done. Also there are no details of the trench on any other sheets in the set. Information, as a minimum, needed: depth, width, backfill material, mix design for asphalt, quantity of conduits (see TA002/6). Please provide section detail showing all necessary items.			
<input type="checkbox"/> ATTACHMENTS:			
<input type="checkbox"/> COST IMPACT:		EST.	<input type="checkbox"/> TIME IMPACT:
CONTRACTOR SIGNATURE: 		DATE: 3.16.23	
RESPONSE:			
<div style="border: 1px solid red; padding: 10px; color: red;">Qty (1) 2" conduit is required per TA101 and TA201. Trench and pull box details are shown on 6,7/TA002. We have no information on existing asphalt - match existing asphalt and base thickness.  Brian Cuneo, PK Electrical, 3/16/2023</div>			
ATTACHMENTS:			
SIGNATURE:		DATE:	

**Darrell Linscott**

---

**To:** Jeff Purcell; Ohm, Ted (Facilities Project Manager)  
**Cc:** Jace Callender; Keven Olsen; Elizabeth Callender  
**Subject:** RE: RTAA Duty Manager Office Bldg Remodel RFI 008

Darrell Linscott  
Project Manager  
**REYMAN BROTHERS CONSTRUCTION INC**  
*"Committed to Your Success"*  
151 S 18<sup>th</sup> Street  
Sparks, NV 89431  
T: 775.356.0150  
F: 775.356.0247  
C: 775.830.6868

**From:** Darrell Linscott  
**Sent:** Friday, March 17, 2023 11:18 AM  
**To:** Jeff Purcell <jeff@paulcavindesign.com>; Ohm, Ted (Facilities Project Manager) <tohm@renoairport.com>  
**Cc:** Jace Callender <jcallender@Reymanbrothers.com>; Keven Olsen <kolsen@Reymanbrothers.com>; Elizabeth Callender <ecallender@Reymanbrothers.com>  
**Subject:** RE: RTAA Duty Manager Office Bldg Remodel RFI 008

Jeff,

The presentation on the documents for the excavation and paving portion of the work is not standard to the industry. The only place on the drawings this sitework is shown is on the T sheets and the only place there is any mention in the spec book regarding this portion of the work is in Division 27. There are no civil sheets. There are no architectural drawings showing this work. There's no way anyone would intuitively go to the T sheets to find excavation and paving or go to Division 27 to find information regarding requirements for excavation and paving. This information is typically found in Division 2. Secondly, the response to the RFI seems to be incomplete. There is no indication as to what type of backfill is to be used, is it to be engineered fill or is the excavated material adequate. What are the specs for the road base or is the excavated material adequate. What is the mix design for the AC paving? All the response says about these issue is that the thickness of the base and paving are to match existing. Once theses issues are adequately addressed, a proper change proposal, if required, can be issued.

Thanks,

P. S.

RBC had an underground locator service locate all the pipe crossings along the new trench rout. There are approximately 50 crossings not including any that may be discovered when NV Energy does their survey.

Darrell Linscott  
Project Manager  
**REYMAN BROTHERS CONSTRUCTION INC**  
*"Committed to Your Success"*  
151 S 18<sup>th</sup> Street  
Sparks, NV 89431  
T: 775.356.0150



3.05

3.05

3.09

3.09

3.01

3.01

3.00

3.00

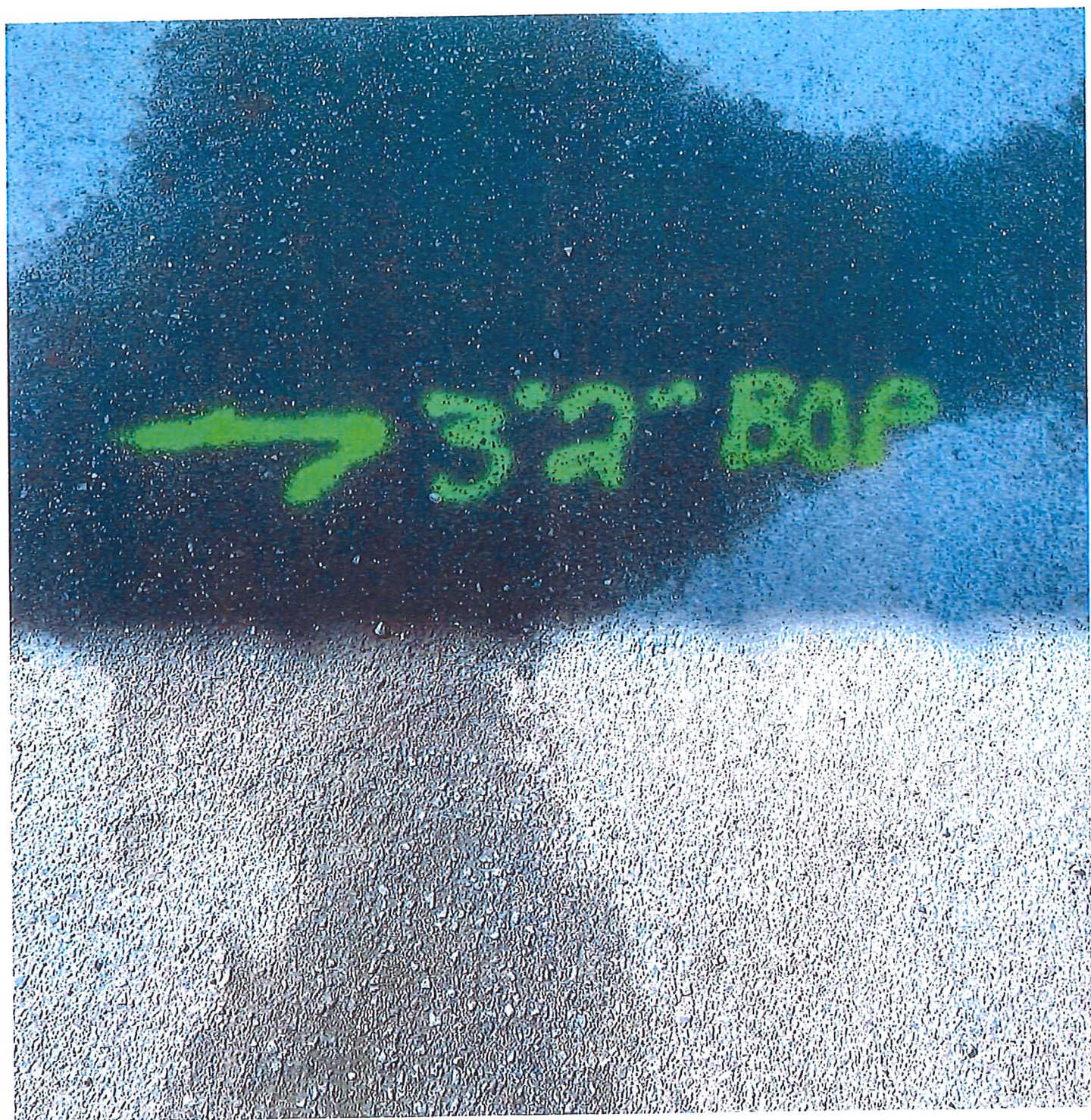
















745 E. Greg St, Suite 3  
Sparks, NV 89431  
O-775-856-9777  
C-775-453-3522

NV A LIC# 0084980 & NV C-5 LIC# 0087471  
mike@westcoastdirtworks.com  
Bid Limit \$1,700,000.00

Date	Proposal No.
3/29/2023	671

Customer Information
Reyman Brothers Construction Elizabeth Callender 151 S 18th St Sparks, NV 89431

Project
1670 National Guard Way

Scope of Work	Qty	Rate	Total
Excavate approximately 300' of trench at 3 1/2 feet deep for electric conduit. Sand trench. All pipe and boxes to be installed by others. Backfill and compact to 95% compaction or better. Asphalt patch excavated trench at 3" thickness. WCDW will provide dust control while working on site.		44,250.00	44,250.00

We Propose hereby to furnish material and labor complete in accordance with the above specifications for the sum of:

**Base Bid: \$44,250.00**

All material is guaranteed to be specified. All work to be completed in a professional manner according to standard practices. Any alteration or deviation from above specifications involving extra costs will be executed only upon written orders, and will become an extra charge over and above the estimate. All agreements beyond Note: Owner to carry fire, tornado and other necessary contingent upon strikes, accidents or delays in insurance. Our workers are fully covered by Workers Compensation Insurance. This proposal is based on day work Mon-Fri 7:00am-3:30pm.

We reserve the right to withdraw this proposal if not accepted within 30 days.

**Exclusions: All Work Unless Specifically Listed Above!**

- After Hours and / or Overtime
- Permits/ Bonds/ Testing of any kind, and all associated fees
- The proposal excludes all layout, unless otherwise outlined above
- Traffic Control
- Concrete Washout
- Frost Protection
- Landscape
- Temp Fencing
- Concrete
- Asphalt Patch
- After Hours Dust Control
- Staking/ Surveying
- Over Excavate Bad Soils
- Irrigation
- Field Measure Prevails
- Retaining Walls
- Shade Structures
- Fencing
- New Turf
- Electrical
- Protect Existing Sod
- Damage to Tree's or Roots
- Trench Drains
- CMU
- Plumbing
- Vapor Barrier
- Striping, Signage
- Cross Drains
- Portable Structures
- Bollards
- Trash Enclosures
- Conduit
- Concrete Pump
- Demo

Owner/Customer shall absorb all costs incurred from unknown conditions such as rock removal, poor digging conditions or poor soil bearing capacity; including but not limited to Excavator, Breaker or Jackhammer plus delivery and operator charges.

Acceptance Of Proposal: If the above prices, specifications and conditions are satisfactory and are hereby accepted. You are authorized to do the work as specified.

DATE OF ACCEPTANCE: \_\_\_\_\_

Authorized  
Signature: \_\_\_\_\_

Title: \_\_\_\_\_



## CONSTRUCTION

"Committed to Your Success"

GENERAL BUILDING CONTRACTORS

4.12.23

### Change Proposal Request

7

HOME OFFICE:  
151 S. 18TH ST.  
SPARKS, NV 89431  
TEL: 775-356-0150  
FAX: 775-356-0247

Paul Cavin Architect, LLC  
**1575 Delucchi Lane**  
Reno NV 89502

re: RTAA Duty Manager Office Bldg. Remodel

Dear Paul,

reymanbrothers.com Presenting the following Change Proposal for review and approval.

Modifications per attached Base Operations Building Claification #2 (see attached)

#### LICENCES:

NEVADA  
#12481C  
  
CALIFORNIA  
#702410

MSM Sheet Metal (see attached)	\$1,300.30
RBC: install countertop and vanity 8 man hrs @ 109.74 / hr	\$877.92
Gardner Engineering (see attached)	\$2,015.00
Triumph Electric (see attached)	\$10,157.64

sub-total	\$14,350.86
OH&P @ 15%	\$2,152.63
<b>TOTAL ADD</b>	<b>\$16,503.49</b>

CONTRACT TIME ADJUSTED:  
Add 10 Callendar Days

Sincerely,

Jace E. Callender  
Project Manager

Approved by: \_\_\_\_\_  
Paul Cavin Design

Date Approved: \_\_\_\_\_



March 31, 2023

## **22025 Reno-Tahoe International Airport – Base Operations Building Improvements – Clarification #2 (Delta 5)**

### **Clarification 2 – Detail D4, Sheet AB801: Casework Elevations, Sections, and Details**

- **Sheet AB801:** This sheet has been revised to add Detail D4 Sink Vanity.

### **Clarification 2 – Sheet MB201: Mechanical Alterations Plan – Base Operations Building**

- **Sheet MB201:** This sheet has been revised to add a Fire Smoke Damper (FSD) at the Breakroom 112.

### **Clarification 2 – Detail 2, Sheet MB701: Temperature Controls**

- **Sheet MB701:** This sheet has been revised to add Detail 2 Combination Fire-Smoke Damper.

### **Clarification 2 – Detail 2, Sheet EB001: Electrical Legend and Drawing Schedule**

- **Sheet EB001:** This sheet has been revised to add Detail 2, Fire Smoke Damper Detail.

### **Clarification 2 – Sheet EB004: Panel Schedule**

- **Sheet EB004:** This sheet has been revised to add #36 Fire/Smoke Damper.

### **Clarification 2 – Sheet EB401: Power Plan**

- **Sheet EB401:** This sheet has been revised to add Fire Smoke Damper.

### **Clarification 2 – Sheet EB501: Fire Alarm Plan**

- **Sheet EB501:** This sheet has been revised to add Fire Smoke Damper.

### **End of Clarification #2**

**Paul Cavin  
Architect LLC**  
1575 Delacorte Lane, Suite 120  
Reno, Nevada 89502  
office: (775) 384-7883  
mobile: (775) 845-0061  
www.paulcavinarchitect.com  
paul@paulcavinarchitect.com



professional seal  
consultant

project

**Reno-Tahoe International Airport  
Base Operations Building Improvements  
PWP #WA-2023-70**  
Reno-Tahoe Airport Authority  
2001 East Plumb Lane  
Reno, Nevada 89502

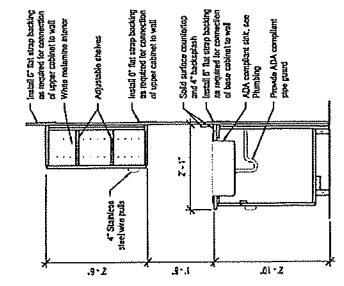
revision	description	date
1	Initial	05/03/23
2	Revised	05/03/23

drawn by: JAVCA/ETB  
checked by: JAVCA  
date: 12/05/22  
project number: 220022  
drawing name: Casework

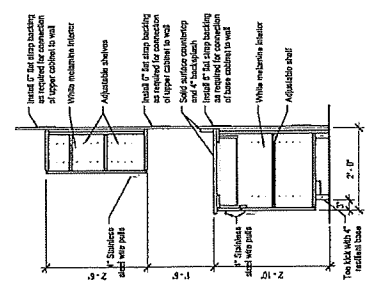
**Casework  
Elevations, Sections  
& Details**

sheet number

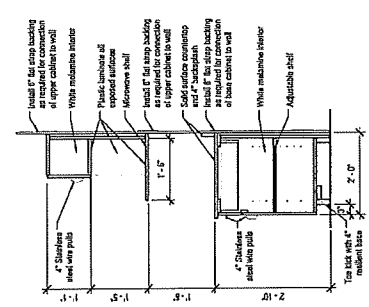
**AB801**



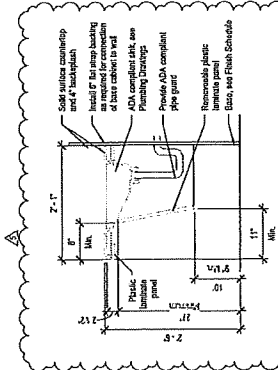
D1 Sink Base and Upper Cabinet Section  
36" x 42"



D2 Base and Upper Cabinet Section  
36" x 42"



D3 Base and Upper Microwave Cabinet Section  
36" x 42"



D4 Sink Vanity  
12" x 15"

MB201

**Mechanical Alterations Floor Plan - Base Operations Building**

**Paul Cavin**  
Architect LLC

1575 Dolucchi Lane, Suite 120  
Reno, Nevada 89502

office: (775) 284-7083  
mobile: (775) 842-0261

www.paulcavindesign.com  
paul@paulcavindesign.com



consultant



**AINSWORTH ASSOCIATES**  
MECHANICAL ENGINEERS  
1429 HOLCOMB AVE., SUITE 201  
RENO, NV 89502  
TEL: 775-259-0100  
4557 GREENVIEW DR  
EL DORADO HILLS, CA 95762  
TEL: 916-777-4014

JCB 2022-014

Reno-Tahoe International Airport Base Operations Building Improvements PWP #WA-2023-70	Reno-Tahoe Airport Authority 2001 East Plumb Lane Reno, Nevada 89502
--	--

[illegible]

drawn by	MM/LLC
reviewed by	AH
date	03/31/22
project number	2202

MB701

## CONTROLS CONTRACTOR

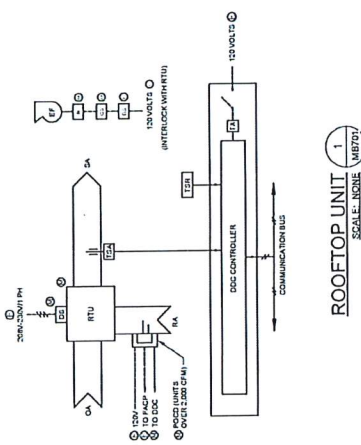
CONTROL SYSTEM SHALL BE DELTA BY EMCOR - (775) 358-0122. TO MATCH EXISTING. NO EXCEPTIONS

## TEMPERATURE CONTROLS LEGEND

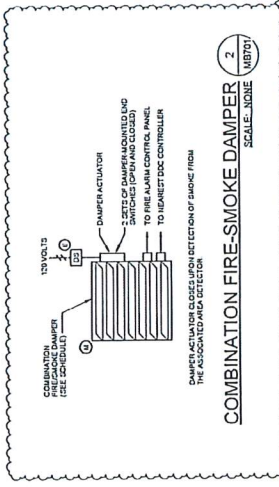
[illegible]

## ELECTRICAL COORDINATION

MECHANICAL TEMPERATURE CONTROL CONTRACTORS SHALL COORDINATE ALL ELECTRICAL POWER REQUIREMENTS GREATER THAN 24 VOLTS WITH THE ELECTRICAL CONTRACTOR.



ROOFTOP UNIT



COMBINATION FIRE-SMOKE DAMPER

### **JAMPER ACTUATOR CLOSES UPON DETECTION OF SMOKE FROM**



[illegible][illegible]

EB001/Net To Scale

[illegible]

<p>— — — — —</p> <p>CONSTRUCTIVE PULL OF ASIDE RELING</p> <p>— — — — —</p> <p>RECOVER FROM POSE OF RELAY ASIDE</p> <p>— — — — —</p> <p>RECOVER FROM CONTINUES</p> <p>— — — — —</p> <p>OVERHEAD SERVICE</p> <p>— — — — —</p> <p>PRIMARY</p> <p>— — — — —</p> <p>SECONDARY</p> <p>— — — — —</p> <p>COMMUNICATION ON SIGNAL</p> <p>— — — — —</p> <p>TELEPHONE</p> <p>— — — — —</p> <p>TELEVISION</p> <p>— — — — —</p> <p>LOW VOLTAGE ASIDE CONTROL, WIRING</p> <p>— — — — —</p> <p>CONSTRUCTIVE PULL OFF WORK AND ON (PITY)</p> <p>— — — — —</p> <p>CONSTRUCTIVE PULL OFF</p>	<p>FLANGED TROTTER</p> <p>— — — — —</p> <p>SURFACE MOUNT TROTTER</p> <p>— — — — —</p> <p>STEP PITCHER</p> <p>— — — — —</p> <p>MAIL PACK</p> <p>— — — — —</p> <p>WIRE MOUNTING UNIT</p> <p>— — — — —</p> <p>WIRE SASH PITCHER - SASHETS AREA OF PITCHETS LIGHTS</p> <p>— — — — —</p> <p>WIRE SASH PITCHER - SASHETS AREA OF PITCHETS LIGHTS</p> <p>— — — — —</p> <p>SHOES RECOVERED TROTTES PITCHER</p>
<p>E = EQUIPMENT</p> <p>M = MOTOR</p> <p>N = NUT</p> <p>S = SCREW</p> <p>W = WIRE</p> <p>Y = YARD</p> <p>Z = ZONE</p>	<p>① THROUGHTS PROVIDED BY ASIDE, CONTRACTION (AND)</p> <p>② ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>③ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>④ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>⑤ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>⑥ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>⑦ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>⑧ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>⑨ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>⑩ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>⑪ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>⑫ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>⑬ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>⑭ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>⑮ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>⑯ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>⑰ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>⑱ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>⑲ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>⑳ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㉑ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㉒ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㉓ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㉔ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㉕ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㉖ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㉗ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㉘ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㉙ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㉚ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㉛ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㉜ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㉝ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㉞ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㉟ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㊱ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㊲ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㊳ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㊴ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㊵ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㊶ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㊷ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㊸ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㊹ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㊺ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㊻ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㊼ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㊽ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㊾ ANCHOR NOT (SEE AS REQUIRED UNITS)</p> <p>㊿ ANCHOR NOT (SEE AS REQUIRED UNITS)</p>

NOTE: NOT ALL DEVICES SHOWN - CONTRACTOR MUST PROVIDE AN UPDATING COMPLETE BUILDING OFFLINE DIAGRAM FOR ALL ASSOCIATED EQUIPMENT AND CONNECTIONS AS THE SYSTEM AS CURRENTLY CONNECTED WITH NEW AND EXISTING DEVICES/EQUIPMENT AS PART OF THEIR DESIGN.

FIRE ALARM SYSTEM ONELINE DIAGRAM NOTES

10097

A		MPS	
1	GROUND/FAULT INTERRUPTER	1	AC FAULT INTERRUPTER
2	WATER VALVE	2	AC FAULT TRIP
3	WATER VALVE	3	AC FAULT TRIP
4	WATER VALVE	4	AC FAULT TRIP
5	WATER VALVE	5	AC FAULT TRIP
6	WATER VALVE	6	AC FAULT TRIP
7	WATER VALVE	7	AC FAULT TRIP
8	WATER VALVE	8	AC FAULT TRIP
9	WATER VALVE	9	AC FAULT TRIP
10	WATER VALVE	10	AC FAULT TRIP
11	WATER VALVE	11	AC FAULT TRIP
12	WATER VALVE	12	AC FAULT TRIP
13	WATER VALVE	13	AC FAULT TRIP
14	WATER VALVE	14	AC FAULT TRIP
15	WATER VALVE	15	AC FAULT TRIP
16	WATER VALVE	16	AC FAULT TRIP
17	WATER VALVE	17	AC FAULT TRIP
18	WATER VALVE	18	AC FAULT TRIP
19	WATER VALVE	19	AC FAULT TRIP
20	WATER VALVE	20	AC FAULT TRIP
21	WATER VALVE	21	AC FAULT TRIP
22	WATER VALVE	22	AC FAULT TRIP
23	WATER VALVE	23	AC FAULT TRIP
24	WATER VALVE	24	AC FAULT TRIP
25	WATER VALVE	25	AC FAULT TRIP
26	WATER VALVE	26	AC FAULT TRIP
27	WATER VALVE	27	AC FAULT TRIP
28	WATER VALVE	28	AC FAULT TRIP
29	WATER VALVE	29	AC FAULT TRIP
30	WATER VALVE	30	AC FAULT TRIP
31	WATER VALVE	31	AC FAULT TRIP
32	WATER VALVE	32	AC FAULT TRIP
33	WATER VALVE	33	AC FAULT TRIP
34	WATER VALVE	34	AC FAULT TRIP
35	WATER VALVE	35	AC FAULT TRIP
36	WATER VALVE	36	AC FAULT TRIP
37	WATER VALVE	37	AC FAULT TRIP
38	WATER VALVE	38	AC FAULT TRIP
39	WATER VALVE	39	AC FAULT TRIP
40	WATER VALVE	40	AC FAULT TRIP
41	WATER VALVE	41	AC FAULT TRIP
42	WATER VALVE	42	AC FAULT TRIP
43	WATER VALVE	43	AC FAULT TRIP
44	WATER VALVE	44	AC FAULT TRIP
45	WATER VALVE	45	AC FAULT TRIP
46	WATER VALVE	46	AC FAULT TRIP
47	WATER VALVE	47	AC FAULT TRIP
48	WATER VALVE	48	AC FAULT TRIP
49	WATER VALVE	49	AC FAULT TRIP
50	WATER VALVE	50	AC FAULT TRIP
51	WATER VALVE	51	AC FAULT TRIP
52	WATER VALVE	52	AC FAULT TRIP
53	WATER VALVE	53	AC FAULT TRIP
54	WATER VALVE	54	AC FAULT TRIP
55	WATER VALVE	55	AC FAULT TRIP
56	WATER VALVE	56	AC FAULT TRIP
57	WATER VALVE	57	AC FAULT TRIP
58	WATER VALVE	58	AC FAULT TRIP
59	WATER VALVE	59	AC FAULT TRIP
60	WATER VALVE	60	AC FAULT TRIP
61	WATER VALVE	61	AC FAULT TRIP
62	WATER VALVE	62	AC FAULT TRIP
63	WATER VALVE	63	AC FAULT TRIP
64	WATER VALVE	64	AC FAULT TRIP
65	WATER VALVE	65	AC FAULT TRIP
66	WATER VALVE	66	AC FAULT TRIP
67	WATER VALVE	67	AC FAULT TRIP
68	WATER VALVE	68	AC FAULT TRIP
69	WATER VALVE	69	AC FAULT TRIP
70	WATER VALVE	70	AC FAULT TRIP
71	WATER VALVE	71	AC FAULT TRIP
72	WATER VALVE	72	AC FAULT TRIP
73	WATER VALVE	73	AC FAULT TRIP
74	WATER VALVE	74	AC FAULT TRIP
75	WATER VALVE	75	AC FAULT TRIP
76	WATER VALVE	76	AC FAULT TRIP
77	WATER VALVE	77	AC FAULT TRIP
78	WATER VALVE	78	AC FAULT TRIP
79	WATER VALVE	79	AC FAULT TRIP
80	WATER VALVE	80	AC FAULT TRIP
81	WATER VALVE	81	AC FAULT TRIP
82	WATER VALVE	82	AC FAULT TRIP
83	WATER VALVE	83	AC FAULT TRIP
84	WATER VALVE	84	AC FAULT TRIP
85	WATER VALVE	85	AC FAULT TRIP
86	WATER VALVE	86	AC FAULT TRIP
87	WATER VALVE	87	AC FAULT TRIP
88	WATER VALVE	88	AC FAULT TRIP
89	WATER VALVE	89	AC FAULT TRIP
90	WATER VALVE	90	AC FAULT TRIP
91	WATER VALVE	91	AC FAULT TRIP
92	WATER VALVE	92	AC FAULT TRIP
93	WATER VALVE		

[illegible]

## DRAWING SCHEDULE

BUDET	DESCRIPTION	BUDET
1000	Operational & General Supplies	0
1001	Public Relations and Publicity	0
1002	Public Services and Publicity	0
1003	Public Information	0
1004	Public Safety	0
1005	Public Health	0
1006	Public Welfare	0
1007	Public Works	0
1008	Public Transportation	0
1009	Public Utilities	0
1010	Public Buildings	0
1011	Public Parks	0
1012	Public Recreation	0
1013	Public Safety	0
1014	Public Health	0
1015	Public Welfare	0
1016	Public Works	0
1017	Public Transportation	0
1018	Public Utilities	0
1019	Public Buildings	0
1020	Public Parks	0
1021	Public Recreation	0
1022	Public Safety	0
1023	Public Health	0
1024	Public Welfare	0
1025	Public Works	0
1026	Public Transportation	0
1027	Public Utilities	0
1028	Public Buildings	0
1029	Public Parks	0
1030	Public Recreation	0
1031	Public Safety	0
1032	Public Health	0
1033	Public Welfare	0
1034	Public Works	0
1035	Public Transportation	0
1036	Public Utilities	0
1037	Public Buildings	0
1038	Public Parks	0
1039	Public Recreation	0
1040	Public Safety	0
1041	Public Health	0
1042	Public Welfare	0
1043	Public Works	0
1044	Public Transportation	0
1045	Public Utilities	0
1046	Public Buildings	0
1047	Public Parks	0
1048	Public Recreation	0
1049	Public Safety	0
1050	Public Health	0
1051	Public Welfare	0
1052	Public Works	0
1053	Public Transportation	0
1054	Public Utilities	0
1055	Public Buildings	0
1056	Public Parks	0
1057	Public Recreation	0
1058	Public Safety	0
1059	Public Health	0
1060	Public Welfare	0
1061	Public Works	0
1062	Public Transportation	0
1063	Public Utilities	0
1064	Public Buildings	0
1065	Public Parks	0
1066	Public Recreation	0
1067	Public Safety	0
1068	Public Health	0
1069	Public Welfare	0
1070	Public Works	0
1071	Public Transportation	0
1072	Public Utilities	0
1073	Public Buildings	0
1074	Public Parks	0
1075	Public Recreation	0
1076	Public Safety	0
1077	Public Health	0
1078	Public Welfare	0
1079	Public Works	0
1080	Public Transportation	0
1081	Public Utilities	0
1082	Public Buildings	0
1083	Public Parks	0
1084	Public Recreation	0
1085	Public Safety	0
1086	Public Health	0
1087	Public Welfare	0
1088	Public Works	0
1089	Public Transportation	0
1090	Public Utilities	0
1091	Public Buildings	0
1092	Public Parks	0
1093	Public Recreation	0
1094	Public Safety	0
1095	Public Health	0
1096	Public Welfare	0
1097	Public Works	0
1098	Public Transportation	0
1099	Public Utilities	0
1100	Public Buildings	0
1101	Public Parks	0
1102	Public Recreation	0
1103	Public Safety	0
1104	Public Health	0
1105	Public Welfare	0
1106	Public Works	0
1107	Public Transportation	0
1108	Public Utilities	0
1109	Public Buildings	0
1110	Public Parks	0
1111	Public Recreation	0
1112	Public Safety	0
1113	Public Health	0
1114	Public Welfare	0
1115	Public Works	0
1116	Public Transportation	0
1117	Public Utilities	0
1118	Public Buildings	0
1119	Public Parks	0
1120	Public Recreation	0
1121	Public Safety	0
1122	Public Health	0
1123	Public Welfare	0
1124	Public Works	0
1125	Public Transportation	0
11	TOTAL BUDGET FOR 2022	0

1575 Delucchi Lane, Suite 120  
Reno, Nevada 89502  
officer: (775) 284-7083  
mobile: (775) 842-0261  
[www.paulcavindesign.com](http://www.paulcavindesign.com)  
[paul@paulcavindesign.com](mailto:paul@paulcavindesign.com)



consultant



**PK Electrical, Inc.**  
*Engineering Design Consulting*  
1425 E. 10th Street, Suite 100, Pomona, CA 91768  
909/786-1100 • Fax 909/786-1101 • E-Mail: [info@pk-electrical.com](mailto:info@pk-electrical.com)

Additional Airport Building Improvements	Authority	ane	02
---	-----------	-----	----

Reno-Tahoe International  
Base Operations Building  
PWP #WA-2023-70

No.	Description	Date
5	CLASSIFICATION #2	3-01-12

### Electrical Legend & Drawing Schedule

Sheet number

EB001

EB004



1575 Delucchi Lane, Suite 120  
Reno, Nevada 89502

1. *Journal of the American Medical Association*, 2000; 283: 2686-2692.

office: (775) 264-7083  
mobile: (775) 842-0764

mobile: (775) 842-0267

[www.paulcaldesign.com](http://www.paulcaldesign.com)

[paul@paulcavindesign.com](mailto:paul@paulcavindesign.com)

ideas, innovation and



consultant

C.

וְהָיָה



ca

ric

cti

00.  
00

E1

K

1

Project

!

nts

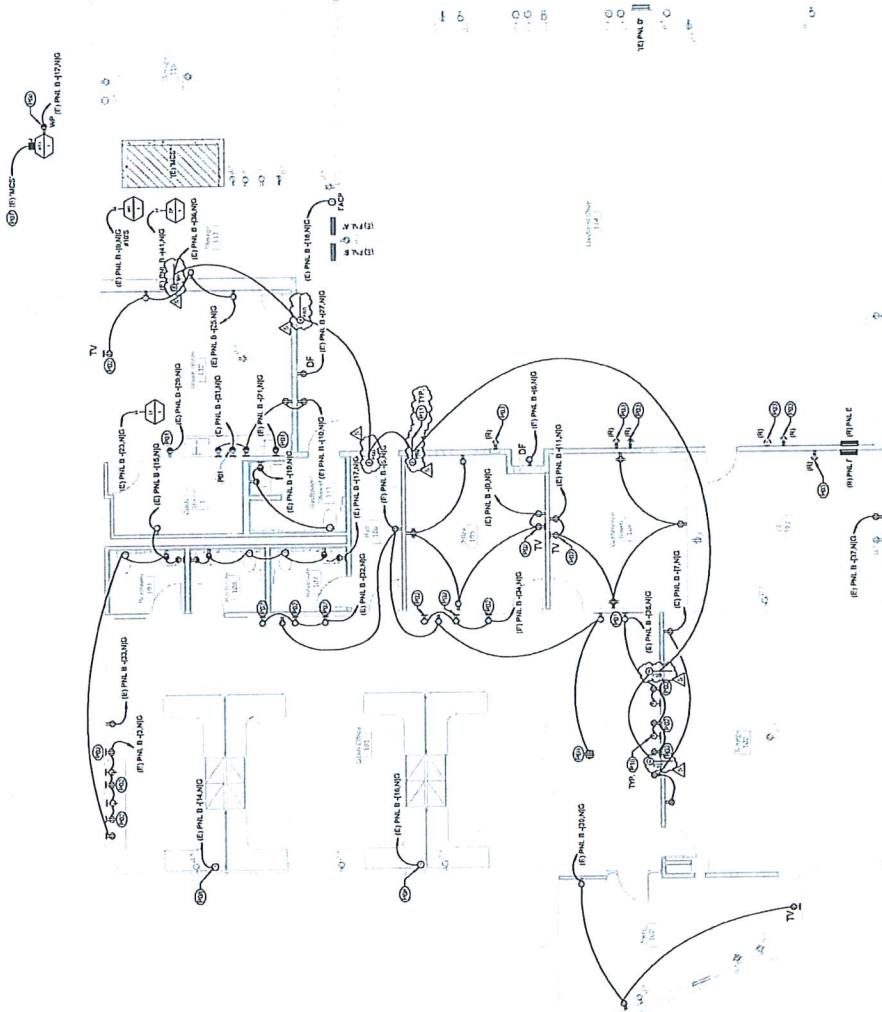
100

## GENERAL NOTES

- [illegible]

## SHEET NOTES

- [illegible]



**Paul Cavin**  
**Architect LLC**  
1575 Delucchi Lane, Suite 120  
Reno, Nevada 89502  
office: (775) 284-7083  
mobile: (775) 842-0261  
[www.paulcavin.design.com](http://www.paulcavin.design.com)  
[paul@paulcavin.design.com](mailto:paul@paulcavin.design.com)



**PK Electrical, Inc.**  
Engineering Design Consulting  
10000 Old Orchard Road, Suite 100, Dallas, TX 75243  
214/343-1111  
www.pk-electrical.com

**project:**

Reno-Tahoe International Airport  
Base Operations Building Improvements  
PWP #WA-2023-70

revisions ☐

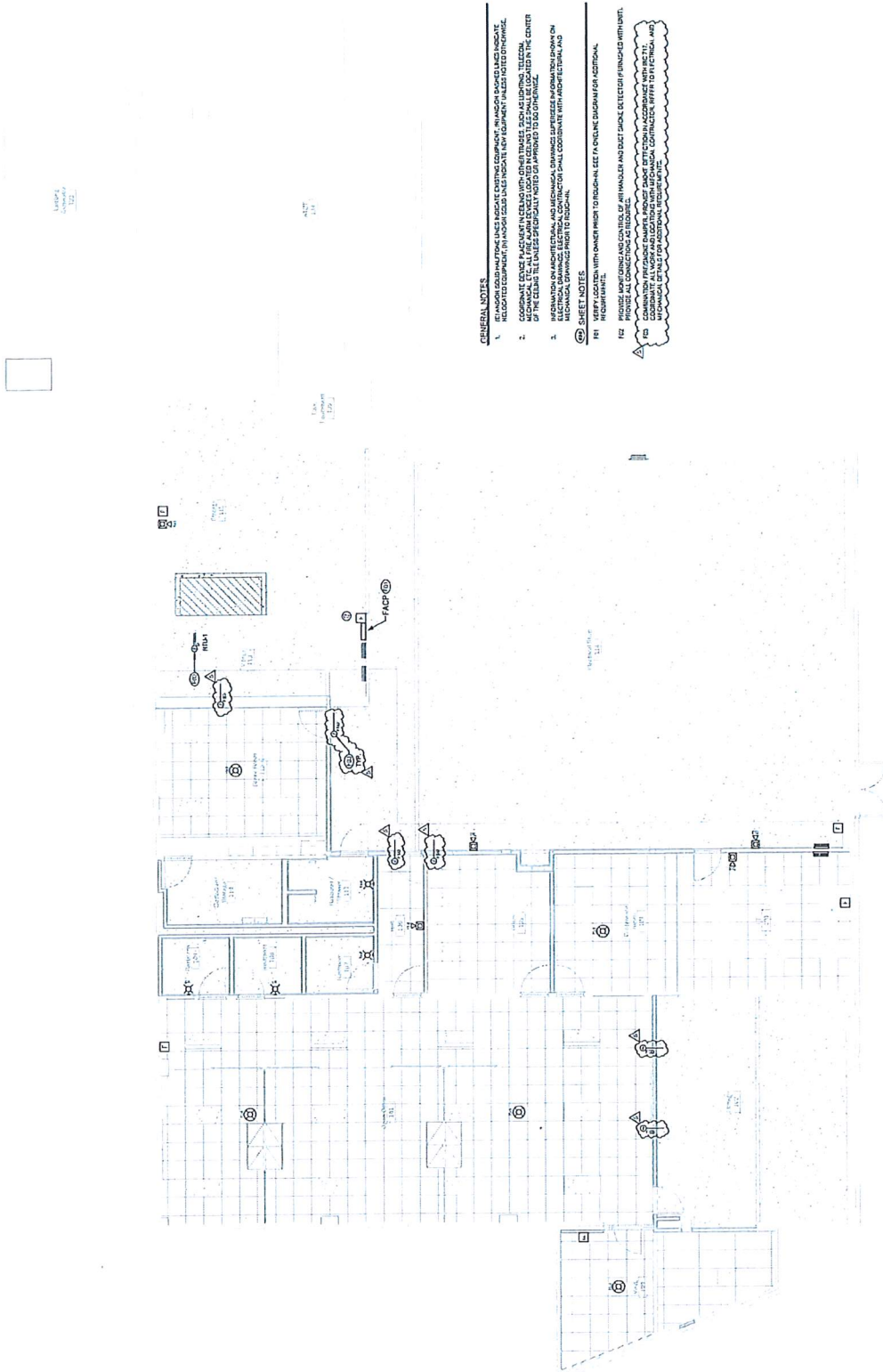
No.	Description	Date
2	PH1401	2/2/23
3	CLASSIFICATION #2	2/2/23

Created By \_\_\_\_\_ TPT  
 Reviewed By \_\_\_\_\_ JEG  
 Date \_\_\_\_\_ 10/28/22  
 Project Number \_\_\_\_\_ 92075

## Fire Alarm Plan

Sheet number

EB501



GENERAL NOTES

1. **COORDINATE SERVICE PLACEMENT IN CEILING WITH OTHER THINGS SUCH AS LIGHTING, TELECOM, AND A/C.** COORDINATE WITH THE ARCHITECTURAL AND MECHANICAL ENGINEERS TO DETERMINE THE CENTER OF GRAVITY OF THE CEILING. THE CENTER OF GRAVITY SHOULD BE APPROVED TO GO ON ONE LEVEL.
2. **COORDINATE SERVICE PLACEMENT IN CEILING WITH OTHER THINGS SUCH AS LIGHTING, TELECOM, AND A/C.** COORDINATE WITH THE ARCHITECTURAL AND MECHANICAL ENGINEERS TO DETERMINE THE CENTER OF GRAVITY OF THE CEILING. THE CENTER OF GRAVITY SHOULD BE APPROVED TO GO ON ONE LEVEL.
3. **COORDINATE SERVICE PLACEMENT IN CEILING WITH OTHER THINGS SUCH AS LIGHTING, TELECOM, AND A/C.** COORDINATE WITH THE ARCHITECTURAL AND MECHANICAL ENGINEERS TO DETERMINE THE CENTER OF GRAVITY OF THE CEILING. THE CENTER OF GRAVITY SHOULD BE APPROVED TO GO ON ONE LEVEL.

## SHEET NOTES

- 181 VERIFY LOCATION WITH OWNER PRIOR TO ROUGH-IN. SEE FA OVERLINE DIAGRAM FOR ADDITIONAL INFORMATION.

F22 PROVIDE MONITORING AND CONTROL OF AIR HANDLER AND DUCT SMOKE DETECTOR (FINISHED WITH UNITS).

PROVIDE ALL CONNECTIONS AS REQUIRED.

# 1 FIRE ALARM PLAN





## REQUEST FOR INFORMATION

PROJECT NAME:  RTAA Duty Manager Office Bldg remodel		RESPOND BY:	ASAP
		GC'S RFI NO.	7
		SUB'S RFI NO.	
TO Paul Cavin Architects LLC		FROM: REYMAN BROS. CONSTRUCTION TEL: 775-356-0150 FAX: 775-356-0247	
RESPONSE BY:			
<input type="checkbox"/> System Eng. Consultants:		<input type="checkbox"/> Mechanical	
<input type="checkbox"/> Structural Eng. Consultants:		<input type="checkbox"/> Plumbing	
<input checked="" type="checkbox"/> Other Consultants:		<input type="checkbox"/> Electrical	
SUBCONTRACTOR: RBC			
SUBJECT: Counter top and sink			
CATEGORY:			
<input checked="" type="checkbox"/> XXX	NEED FOR CLARIFICATION	<input type="checkbox"/>	OMISSION
<input type="checkbox"/>	UNFORSEEN CONDITION	<input type="checkbox"/>	COORDINATION PROBLEM
<input type="checkbox"/>	CONFLICT WITHIN DOCUMENTS	<input type="checkbox"/>	OTHER
<u>SPEC. SECTION</u>	<u>PARAGRAPH NO.</u>	<u>DRAWING NO.</u>	<u>DETAIL NO.</u>
DESCRIPTION: Please see attached. These drawings show a countertop & backsplash. The approved and ordered sink for this space is a undermount sink (see L2 in the schedule). How is the countertop and sink to be supported?			
<div style="border: 1px solid red; padding: 5px; background-color: #e0ffe0;">Countertop and sink will be supported on plastic laminate vanity per upcoming Clarification #02. See attached detail.  Jeffrey Purcell Paul Cavin Architect LLC 03/28/23</div>			
<input type="checkbox"/> ATTACHMENTS:		<input type="checkbox"/> TIME IMPACT:	
<input type="checkbox"/> COST IMPACT:		EST.	
CONTRACTOR SIGNATURE: <i>Jeffrey Purcell</i>		DATE: 3.15.23	
RESPONSE:			
ATTACHMENTS:			
SIGNATURE:		DATE:	

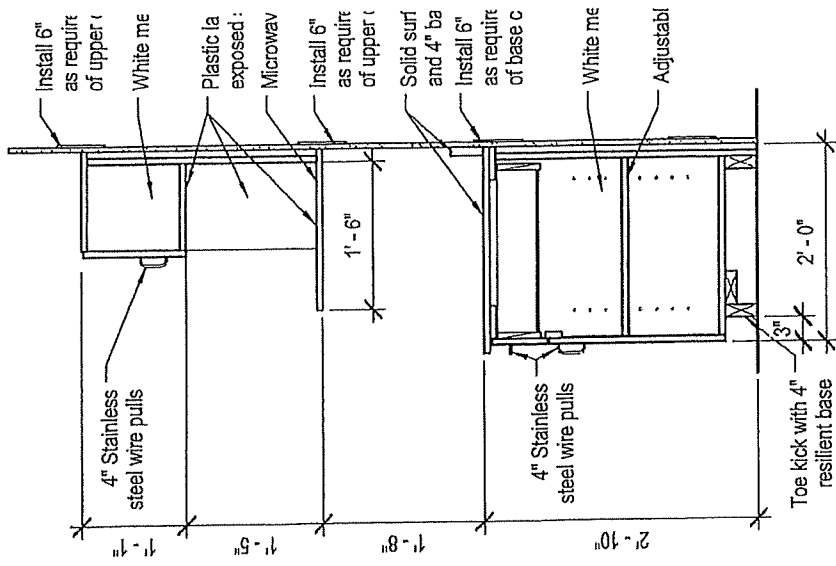
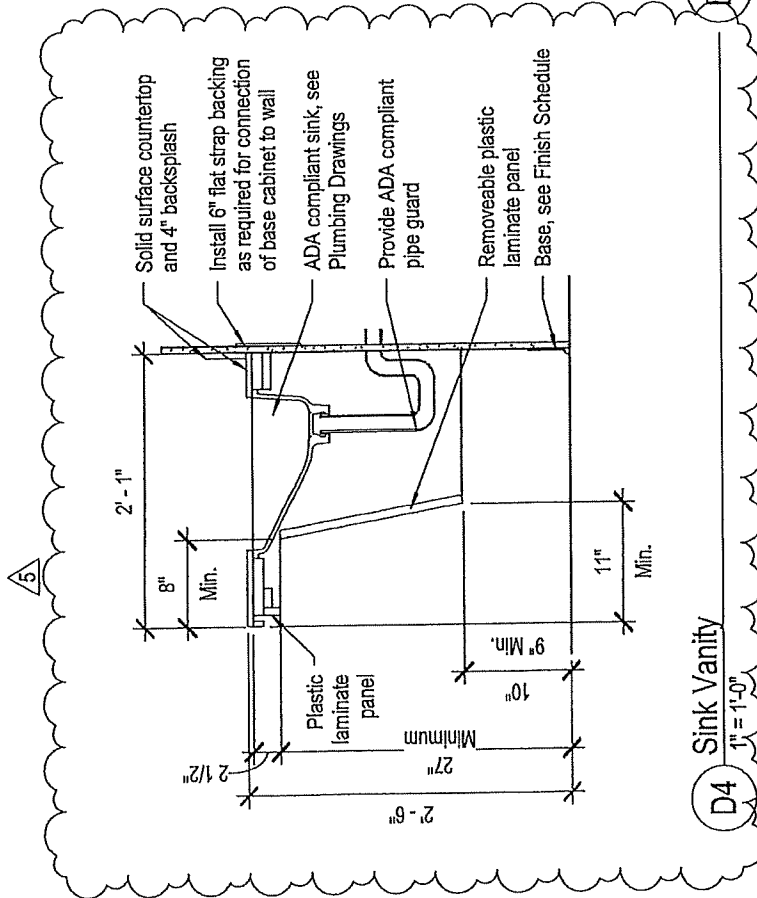




\_\_\_\_\_







Base and Upper Microwave Cabinet Section

**SHEET METAL & STEEL FABRICATION INC**

1100 East Greg Street Sparks, NV 89431

WWW.MSMSHEETMETAL.COM

(775) 356-5542 Fax (775) 356-8927

**Quote #49029.001****Date:** 3/31/2023**To:**

Reyman Bros. Construction

**Attn:**

Darrell Linscott

**Project:**RTAA Duty Manager Office Remodel -  
CHANGE ORDER REQUEST**Delivery Address:**Reno Tahoe International Airport  
Base Operations and MO Building  
2001 East Plumb Lane  
Reno, NV 89502

Sales Person: Chris Moore		Ship Via: MSM	Terms:	Net 25th	
Qty	Description	Unit Price		Amount	
1.000	Add Vanity Support Brackets and Countertop at Restroom/Shower 111; Per RFI-007. *NOTE: PENDING PHYSICAL CONFIRMATION OF SINK PROFILE TO DETERMINE ADA CLEARANCES	\$1177.94		\$1177.94	

**THIS QUOTE IS SUBJECT TO PRICE AND AVAILABILITY AT TIME OF ORDER**

This is a quotation on the goods named, subject to the conditions noted below:  
1) Subject to change with additions or subtractions, change of scope, or material changes.  
2) MSM is only a supplier, we do NOT install or fabricate on site.  
3) MSM is NOT responsible for any lost, stolen, or damaged material after it has been delivered.

Prepared By Chris Moore	<b>Subtotal</b>	\$1,177.94
	<b>Tax</b> <sup>Reno</sup> 8.265%	\$97.36
	<b>Freight</b>	\$25.00
	<b>Total</b>	\$1,300.30

To accept this quotation, sign here and return: \_\_\_\_\_



# MSM

## SHEET METAL & STEEL FABRICATION

MSM Millwork  
1100 E. Greg St.  
Sparks, NV 89431  
Phone: 775-356-5542  
Fax: 775-356-8927  
Email: [millwork@msmsheetmetal.com](mailto:millwork@msmsheetmetal.com)

To:  
Elizabeth Callender  
Reyman Brothers Construction  
RTAA Duty Manager Office Remodel

### CHANGE ORDER REQUEST

Quotation #: 49029.001  
Date: March 31, 2023

MSM will provide shop drawings, per customer spec and contractor provided dimensions, subject to two (2) revisions, or a combined 12 CAD hours total. Additional revisions may result in additional CAD time charges. Changes of specified materials and/or revisions will require a written and signed change order. Contractor is responsible to verify field conditions and dimensions. Proposal is for millwork and associated hardware only; internal conduit, electrical prewire, electrical devices, plumbing fixtures, etc., are not included. **Installation is the responsibility of the Contractor.** Countertop breaks, field joints, and field assembly will be noted on shop drawings. Where not specified, plastic laminate cabinetry and countertops will be Wilsonart® "Frosty White" 1573-60. Alternate laminate selection may require a change order for additional material costs. Cabinet construction to be rabbet and dado, on particle board core, w/ melamine interiors and plastic laminate on exposed surfaces. Drawer boxes to be 1/2" Baltic Birch w/ full extension sidemount slides. MSM is not a member of AWI's QCP program. Any AWI inspections to occur outside of the scope of this quote. Quote is based off of drawings and specifications by Paul Cavin Architect, LLC, dated 10/28/2022. Casework package to include:

- PL-01 - Wilsonart® "Frosty White" 1573-60
- SS-01 - Corian® Group 3 or Lower
- White Melamine Interiors
- Plastic Laminate on Exposed Surfaces
- Matching PVC Edgebanding
- European 110° Concealed Hinges
- Accuride 3832E Side Mount Drawer Slide, Or Equal
- Palletized Curbside Delivery

#### Excludes:

- Jobsite Offloading
- Installation
- Field Dimensioning
- Backing
- AWI Certification or Inspections
- All Other Items not Expressly Listed Above

Please See Following Pages for Detailed Product Scope:

**RTAA Duty Manager Office Remodel**  
**31-Mar-23**

**Description**

● **Breakroom 112 - B1/A753**

3.25 LF	PL-01_Base Cabinets_Front Apron
1 EA	PL-01_Base Cabinets_Removable Front Panel
2 EA	Stainless Steel ADA Vanity Side Bracket
8.125 SF	SS-01_Solid Surface Countertop_Loose Backsplash

Total	<b>\$1,177.94</b>
-------	-------------------





270 East Parr Boulevard Reno, Nevada 89512  
ph (775) 329-4133 • fx (775) 329-4564 • [www.gardnereng.com](http://www.gardnereng.com)

April 4, 2023

Reyman Brothers Construction  
151 South 18th street  
Sparks, Nevada 89431  
Phone (775) 356-0150  
Fax (775) 356-0247

Attention Mr. Darrell Linscott  
Project Manager  
[dlinscott@reymanbrothers.com](mailto:dlinscott@reymanbrothers.com)

Reference RTAA Duty Manager Office Remodel  
GEI Job # 6638 – CE #3

Subject Added FSD in Breakroom  
Clarification #2

Dear Darrell;

The following are our cost estimates for the addition of a FSD with actuator and auxiliary switch. We have not included electrical, or fire alarm work of any sort. Please see the attached spreadsheet for an explanation of all associated costs.

*Total costs for this scope of work.....\$2,015.00  
(Two Thousand Fifteen Dollars)*

This change estimate is based solely on direct cost elements such as labor, materials and normal markups, and does not include any amount for changes in sequence of work, delays, disruptions, rescheduling, extended overhead, acceleration, wage, material or other escalations beyond the prices upon which this proposal was based, or any other impact costs. The right is expressly reserved, and notice of potential claim is made, for any and all of these and other related items of cost prior to any final payment under this contract.

Please feel free to contact us if you have any questions or need any additional information.

Thank you,

**GARDNER ENGINEERING INC.**

*D. Robinson*

Danny Robinson  
Project Manager  
(775) 329-4133

GARDNER ENGINEERING, INC.

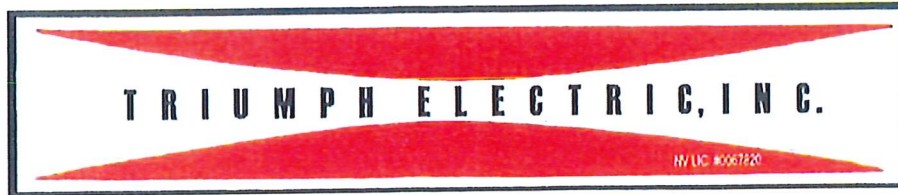
Project Name:  
Change Estimate #  
Date:

RTAA Duty Manager Office Remodel

CE #3 Added FSD in Breakroom

April 4, 2023

Description	Material Quantity	Shop Labor	Field Labor	Unit Cost	Total	Category Total
<b>Materials</b>						
12" FSD with actuator	1.00			1,080.00	1,080.00	
Sleeve adapters	2.00			75.00	150.00	
					-	
					-	
					-	
						1,230.00
<b>Labor</b>						
Fabrication of sleeves and Installation of FSD		2.00	2.00	105.00	420.00	
					-	
					-	
					-	420.00
<b>Subcontracts and Other Costs</b>						
*****EXCLUDES AND FIRE ALARM WORK OF ANY KIND*****					-	
					-	
					-	
					-	-
Materials						1,230.00
Equipment						-
Sales Tax					8.265%	102.00
Labor						420.00
Subcontracts and Other Costs						-
Subtotal						1,752.00
Overhead & Profit					15.00%	263.00
Bond						-
Change Estimate Total						2,015.00



1360 Greg Street Suite 106  
Sparks, Nevada 89431

NV License# 0067820  
Bid Limit: UNLIMITED

Fax (775)-355-1977  
Phone (775)-355-1965

**COR #3**

CHANGE ORDER REQUEST		
Proposal Submitted To: RBC	Phone:	Fax:
Date: Monday, April 10, 2023	Job Name: RTAA Clarification #2	Job Location: Reno NV

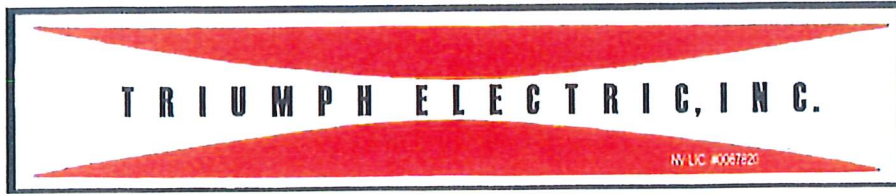
**SCOPE OF WORK TO PERFORM**

Per Clarification #2 #2

1. Provide 120v power to (6) FSD's provided and installed by others.
2. Provide Fire alarm control to (6) FSD's.
3. Provide Fire alarm redesign, programming, testing and equipment.

Attachments:

1. Bid Summary
2. L-M Breakdown



1360 Greg Street Suite 106  
Sparks, Nevada 89431

NV License# 0067820  
Bid Limit: UNLIMITED

Fax (775)-355-1977  
Phone (775)-355-1965

EXCLUSIONS	
-	Cut, patch and paint
-	Temp power and lighting
-	Bonds
-	Overtime
-	Provide and install of FSD's

PRICE	
Price	\$10,157.64

Respectfully Submitted By, Ryan Flickinger

ACCEPTANCE OF PROPOSAL

The above scope of work, exclusions, prices, specifications and conditions are satisfactory and are hereby accepted

Date:

Print Name:

Signature:



RTAA Duty Managers office Remodle : Clarification #2  
Totals (Summary) - Bid Summary: Default

Material	
Non-Quoted	\$931.35
Quotes	0.00
Sales Tax (8.27%)	76.98
Total Material	\$1,008.33
Labor	
Direct (41.52 hours @ \$95.00)	\$3,944.40
Non-Productive Labor	380.00
Total Labor (45.52 hours)	\$4,324.40
Direct Job Expenses	\$0.00
Tools and Miscellaneous Materials	0.00
Subcontracts Delta Fire cost	3,500.00
Job Subtotal (Prime Cost)	\$8,832.73
Overhead (0.00%)	0.00
Profit (15.00%)	1,324.91
Job Total	\$10,157.64
Actual Bid Price	\$10,157.64
Material to Direct Labor ratio: 0.20	
Prime Cost per square foot	\$0.00
Job Total per square foot	\$0.00
Actual Bid Price per square ft	\$0.00
Labor cost per square foot	\$0.00
Labor hours per square foot	0.00
Gross Profit %	13.04
Gross Profit \$	\$1,324.91
Net Profit %	13.04

## RTAA Duty Managers office Remodle : Clarification #2

Job Number: CO1242-3

Bid Summary: Default

Extension By Phase

Item #	Description	Quantity	Price U	Ext Price	Labor Hr U	Ext Lab Hr
--- 01 Raceway, Fittings & Boxes ---						
1000	1/2" EMT	150	55.00 C	82.50	4.50 C	6.75
1001	3/4" EMT	150	95.01 C	142.52	5.00 C	7.50
1121	1/2" Steel Flex	36	75.00 C	27.00	3.00 C	1.08
1456	1/2" Set Screw Steel Insul Throat Conn	16	24.99 C	4.00	0.05 E	0.80
1457	3/4" Set Screw Steel Insul Throat Conn	12	50.01 C	6.00	0.06 E	0.72
1556	1/2" Set Screw Steel Cplg	15	25.00 C	3.75	0.05 E	0.75
1557	3/4" Set Screw Steel Cplg	15	50.00 C	7.50	0.06 E	0.90
1894	1/2" Steel Squeeze Flex Conn	24	94.11 C	22.59	0.09 E	2.16
2278	1/2" 1-Hole Strap	33	10.00 C	3.30	4.29 C	1.42
2279	3/4" 1-Hole Strap	21	20.00 C	4.29	2.29 C	0.49
2555	3/8" Self Drill Anchor	54	10.00 C	5.44	3.00 C	1.63
2571	4" Square Box (1/2 & 3/4 KO's)	2	205.01 C	4.10	0.18 E	0.36
2573	4" Square x 2-1/8" Deep Box (1/2 & 3/4 KO's)	6	200.04 C	12.00	0.18 E	1.08
2574	4" Square x 2-1/8" Deep Box w/brkt (1/2&3/4 KO's)	6	249.92 C	15.00	0.20 E	1.20
2731	4" Square-1G Plaster Ring-5/8"D	6	99.99 C	6.00	0.07 E	0.42
--- 01 Raceway, Fittings & Boxes Total ---				345.99		27.26
--- 02 Wire & Cable ---						
2787	#12 THHN CU Solid Wire	630	220.00 M	138.60	6.50 M	4.10
11570	16/2 AWG Unshielded Fire Alarm Wire FPLR	300	205.00 M	61.50	10.00 M	3.00
--- 02 Wire & Cable Total ---				200.10		7.10
--- 03 Distribution ---						
5585	1P Manual Starter-Nema 1	6	58.14 E	348.84	0.40 E	2.40
--- 03 Distribution Total ---				348.84		2.40
--- 05 Wiring Device & Covers ---						
4716	4" Square 1G Ind Switch Cover	6	200.00 C	12.00	0.10 E	0.60
4791	4" Square Flat Blank Cover w/KO	2	60.01 C	1.20	0.07 E	0.14
--- 05 Wiring Device & Covers Total ---				13.20		0.74
--- 07 Misc ---						
6839	Red Wirenuts	51	220.00 M	11.22	2.00 C	1.02
T0001	Hook up damper	6	1.00 E	6.00	0.25 E	1.50
T0002	Hook up FA control relay	6	1.00 E	6.00	0.25 E	1.50
--- 07 Misc Total ---				23.22		4.02
Job Total				931.35		41.52



## CONSTRUCTION

"Committed to Your Success"

GENERAL BUILDING CONTRACTORS

4.28.23

### Change Proposal Request

8

#### HOME OFFICE:

151 S. 18TH ST.  
SPARKS, NV 89431  
TEL: 775-356-0150  
FAX: 775-356-0247

Paul Cavin Architect, LLC  
**1575 Delucchi Lane**  
Reno NV 89502

re: RTAA Duty Manager Office Bldg. Remodel

Dear Paul,

Presenting the following Change Proposal for review and approval.

Modifications per attached Base Operations Building Claification #3 (see attached)

#### LICENCES:

NEVADA  
#12481C

CALIFORNIA  
#702410

Gardner Engineering (see attached)

\$2,100.00

Classic Finishes (see attached)

\$1,582.00

sub-total	\$3,682.00
OH&P @ 15%	\$552.30
<b>TOTAL ADD</b>	<b>\$4,234.30</b>

CONTRACT TIME ADJUSTED:  
Add 5 Callendar Days

Sincerely,

Jace E. Callender  
Project Manager

Approved by: \_\_\_\_\_  
Paul Cavin Design

Date Approved: \_\_\_\_\_

April 24, 2023

**22025 Reno-Tahoe International Airport – Base Operations Building Improvements –  
Clarification #3 (Delta 6)**

**Clarification 3 – Sheet AB511: Alteration Reflected Ceiling Plan–Base Operations Building**

- **Sheet AB511:** This sheet has been revised to indicate the required modifications to the new lay-in ceiling design within the Vestibule 100 space due to a conflict with the mechanical duct routing and an existing rainwater leader pipe. The (2) lay-in ceilings that were previously shown will now be a single ceiling at 10'-0" a.f.f. The stud framed header that was added in Clarification #2 has now been removed. A slope transition within the lay-in ceiling system has been added at the existing storefront entry system. Mechanical modifications are kept to a minimum with the previously specified offset duct being flipped over to allow for the duct to go lower as well as a register being relocated which is accomplished by the previously designed flex duct. See the mechanical modification description below.

**Clarification 3 – Sheet MB201: Mechanical Alterations Plan–Base Operations Building**

- **Sheet MB201:** This sheet has been revised to indicate the previously designed offset duct to be flipped over allowing for the remainder of the ducting to be located beneath the existing rainwater leader piping. The removal of the 16/12 offset duct located within the Vestibule 100 space is also indicated.

**End of Clarification #3**

**Attachments:**

**AB511: Alteration Reflected Ceiling Plan – Base Operations Building**  
**MB201: Mechanical Alterations Plan – Base Operations Building**

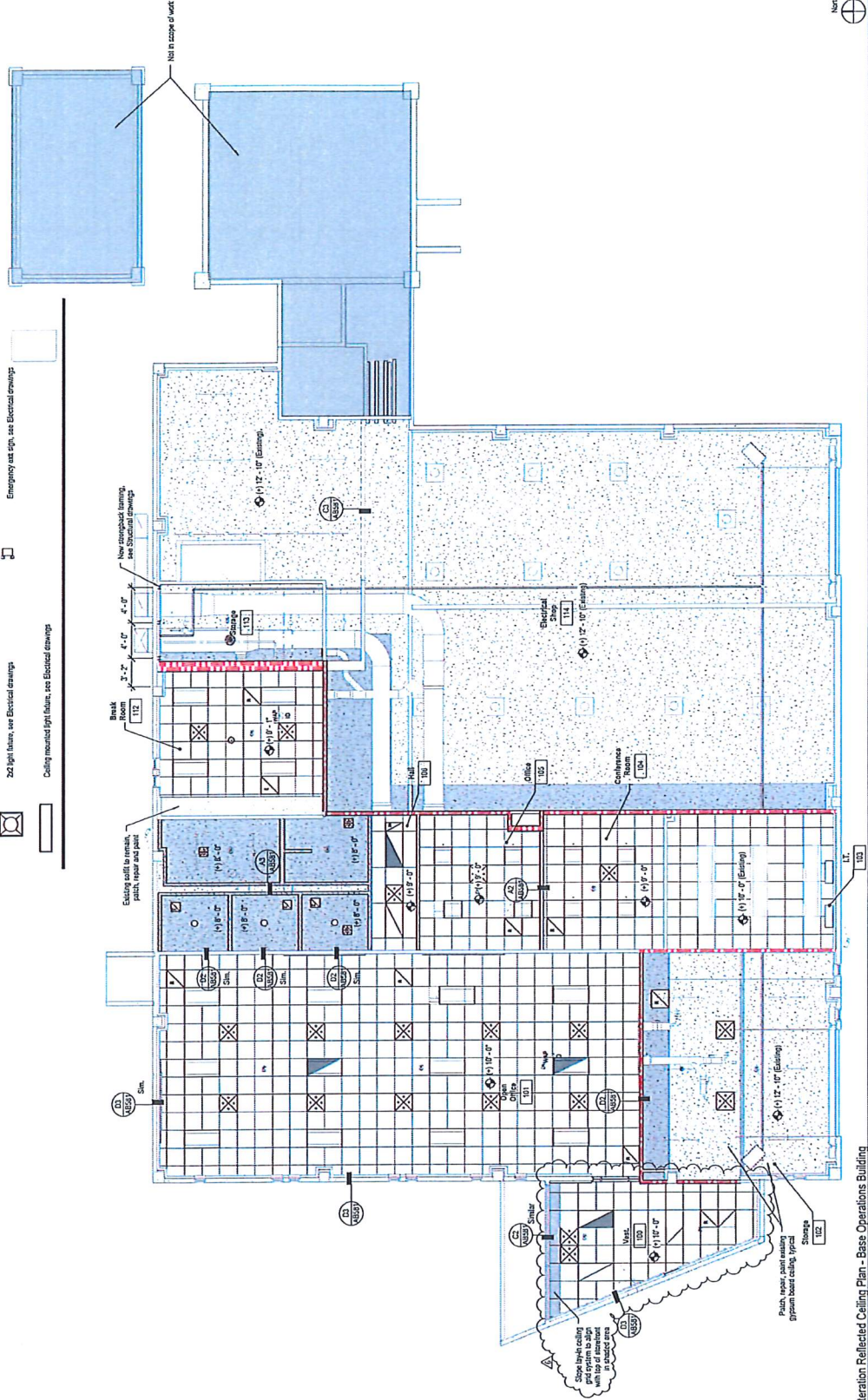


# Reflected Ceiling Plan Legend

- 2x4 lay-in ceiling grid system, see Specifications
- 10' Type 'X' gypsum board ceiling, see Specifications (New ceiling board number)
- Open to structure above
- 2x4 light fixture, see Electrical drawings
- 2x2 light fixture, see Electrical drawings
- Ceiling mounted light fixture, see Electrical drawings
- 4x4 lay-in ceiling grid system, see Specifications
- 10' Type 'X' gypsum board ceiling, see Specifications (New ceiling board number)
- Open to structure above
- 2x4 light fixture, see Electrical drawings
- 2x2 light fixture, see Electrical drawings
- Ceiling mounted light fixture, see Electrical drawings
- As supply register, see mechanical drawings
- As return grille, see mechanical drawings
- Smoke detector
- Pipe support
- Emergency exit sign, see Electrical drawings

# Reflected Ceiling Plan Notes

- Contractor shall comply with IBC, local codes and manufacturers written restrictions and recommendations for installation of lay-in ceiling.
- See Project Manual and Specifications for additional information and requirements.
- See Mechanical drawings and Specifications for additional information and requirements.
- See Electrical drawings and Specifications for additional information and requirements.
- See Sheet R501 for additional information.



05 Alteration Reflected Ceiling Plan - Base Operations Building

3/15' x 15'

**Paul Cavin Architect LLC**

1575 Delacchi Lane, Suite 120  
Reno, Nevada 89502  
office: (775) 254-7083  
mobile: (775) 842-0261  
www.paulcavinarchitect.com  
paul@paulcavinarchitect.com



consultant

project

Reno-Tahoe International Airport  
Base Operations Building Improvements  
PWP #WA-2023-70  
2001 East Plumb Lane  
Reno, Nevada 89502

Rev	Description	Date
1	Construction	12/1/23

Drawn by	JANIS/MLB
Checked by	MLB
Date	12/05/23
Project number	22022
Sheet name	

Alteration Reflected Ceiling Plan - Base Operations Building

sheet number

**AB511**

MB201



**Mechanical Alterations Floor Plan - Base Operations Building**





270 East Parr Boulevard Reno, Nevada 89512  
ph (775) 329-4133 • fx (775) 329-4564 • [www.gardnereng.com](http://www.gardnereng.com)

April 27, 2023

Reyman Brothers Construction  
151 South 18th street  
Sparks, Nevada 89431  
Phone (775) 356-0150  
Fax (775) 356-0247

Attention Mr. Darrell Linscott  
Project Manager  
[dlinscott@reymanbrothers.com](mailto:dlinscott@reymanbrothers.com)

Reference RTAA Duty Manager Office Remodel  
GEI Job # 6638 – CE #4

Subject Duct Offset at Roof Drain  
Clarification #3 (Delta #6)

Dear Darrell;

The following are our cost estimates for the described offset of the two ducts into the lobby area of the operations building. This is detailed in the above mentioned clarification, dated 4/27/2023 Please see the attached spreadsheet for an explanation of all associated costs.

**Total costs for this scope of work.....\$2,100.00**  
**(Two Thousand One Hundred Dollars)**

This change estimate is based solely on direct cost elements such as labor, materials and normal markups, and does not include any amount for changes in sequence of work, delays, disruptions, rescheduling, extended overhead, acceleration, wage, material or other escalations beyond the prices upon which this proposal was based, or any other impact costs. The right is expressly reserved, and notice of potential claim is made, for any and all of these and other related items of cost prior to any final payment under this contract.

Please feel free to contact us if you have any questions or need any additional information.

Thank you,

**GARDNER ENGINEERING INC.**

*D. Robinson*

Danny Robinson  
Project Manager  
(775) 329-4133

GARDNER ENGINEERING, INC.

Project Name:

Change Estimate #

Date:

RTAA Duty Manager Office Remodel

CE #4 - Duct Offset at Roof drain

April 27, 2023

Description	Material Quantity	Shop Labor	Field Labor	Unit Cost	Total	Category Total
<b>Materials</b>						
Rectangular Duct Fittings (16x12)	4.00			96.00	384.00	
Round Offset fittings (14")	4.00			83.00	332.00	
					-	
					-	
					-	
						716.00
<b>Labor</b>						
Fab and Install of offset		4.00	6.00	105.00	1,050.00	
					-	
					-	
					-	1,050.00
<b>Subcontracts and Other Costs</b>						
					-	
					-	
					-	
					-	-
	Materials					716.00
	Equipment					-
	Sales Tax				8.265%	60.00
	Labor					1,050.00
	Subcontracts and Other Costs					-
	Subtotal					1,826.00
	Overhead & Profit				15.00%	274.00
	Bond					-
	Change Estimate Total					2,100.00

775.470.5218 Office  
775.470.5303 Fax



Nevada Contractors  
Licenses:  
#80455 • C4  
#80454 • C17

708 N. Center Street • Reno, NV 89501

April 26, 2023

Reyman Brothers Construction  
151 S 18th St  
Sparks, NV 89431

(775) 356-0150

Re: Reno-Tahoe International Airport Base Operations and Airfield Maintenance Office Improvements.

To whom it may concern,

We are pleased to provide you with our estimate to furnish and install the following services and materials for the above-referenced project. We are signatory to our respective trade unions, fully licensed, and bondable at a rate of approximately 2.50%. Please review the following for scope and exclusions.

This estimate is based on Clarification 3 dated 04-24-2023.

Work Included:

- Furnish and Install USG's Hold Down Clips, USG's SB2 Adjustable Wall Molding Brackets, and USG's MAC2 Cross Tee Clips per USG's Seismic Technical Guide for Sloped Ceilings, for Sloped Ceiling in Vestibule 100 per AB511 received in part with Clarification 3.

**Total Clarification #3 Estimate (including tax): \$1,582.00**





Work Excluded:

- \*\*Demolition
- \*\*Wood, Plywood, Wood Blocking/Firestopping, and or wood carpentry.
- \*\*Metal, Aluminum, and/or Plastic Corner Guards.
- \*\*Access Panels and/or installation of.
- \*\*Aluminum Brake Metal, and/or any Extruded Metal Products to match Storefronts or Windows.
- \*\*Metal Flashing & Copings.
- \*\*110 & 220 3 phase power sources.
- \*\*Grouting hollow metal door frames.
- \*\*Color coding, I.D. banding sprinklers pipe painting.
- \*\*Striping and signage paint.
- \*\*Concrete floor sealers/hardeners.
- \*\*Repair and patching Classic Finishes scope of work, due to damage by others.
- \*\*Electrical/mech/plumbing fixture support wires.
- \*\*Fire rating light fixtures and electrical outlets.
- \*\*Firesafing, fire-caulking, firestopping.
- \*\*Sound batt and/or Thermal Insulation.
- \*\*Caulking/sealants.
- \*\*Classic Finishes standard exclusions consist of the following:
  - ##Overtime (Over 40 Hours/Week); Weekends, Nightwork, or Shiftwork.
  - ##Temporary heat or weather protection.
  - ##Lighting, task, and area type.
  - ##All water requirements, minimum at 50 psi.
  - ##Testing, permits, bonds, fees, grade setting.
  - ##Temporary facilities, barriers, enclosures, and waste management.
  - ## No shop drawings, design build, as-built drawings, engineering, or calculations.

All work proposed must be ready for continuous progress and in-sequence work. Costs incurred by Classic Finishes, due to the delay by others shall be billed to General Contractor accordingly.

Should Classic Finishes be awarded this project, our bid Proposal in its entirety shall be included as an exhibit to the contract, as it is specific to our scope. Our Scope is limited to the above referenced scope inclusions and exclusions, no other assumptions have been made.

We thank you for the opportunity to submit an estimate for the above upcoming project. If you have any questions or comments, please feel free to call.

Classic Finishes, LLC Nevada Licenses:

- 1) #80455-C4: Limit: Unlimited 2) #80454-C17: Limit: Unlimited

Respectfully submitted,

Lorenzo Taormina  
Estimating

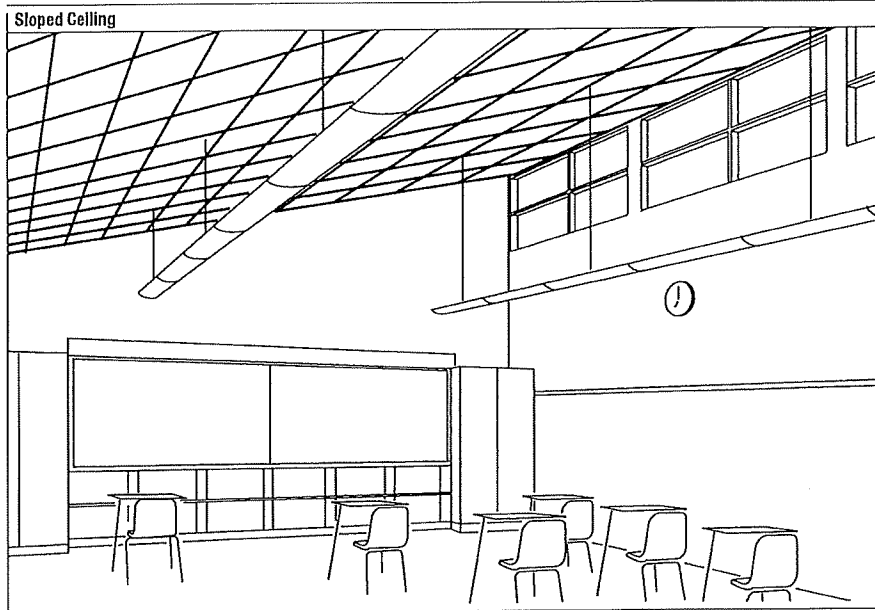
copy: rg

Quoted prices valid for 30 days unless otherwise noted. All material is guaranteed to be as specified. All work to be completed in a workmanlike manner, according to standard practices. Any alteration or deviation from above specifications involving extra cost will be executed only upon written orders and will become an extra charge over and above the proposal. All agreements contingent upon strikes, accidents, or delays beyond our control. Our workers are fully covered by worker's compensation Insurance. Issuance of subcontract deems acceptance to all conditions set forth.

# Seismic Technical Guide

## Sloped Ceilings

Designing and installing a sloped suspended ceiling can contribute to LEED® EQ Credit 8.1 Daylighting and Views. This design philosophy provides the building occupants a connection between indoor spaces and the outdoors through the introduction of daylight and views into the regularly occupied areas of the building. Generally, a sloped suspended ceiling and its supports and attachments are installed and designed in the same manner as a level suspended ceiling. However, there are exceptions and the actual construction of a sloped suspended ceiling in a seismic design category can meet code requirements in different ways.



USG® has a long history of product development and innovation for suspended ceiling systems in seismic applications and continues to commit resources to this endeavor. We have thoroughly examined lay-in sloped ceilings for seismic design categories C-F. The findings of this study and our recommendations are presented in this technical guide to assist in the interpretation of this ceiling application. There are many factors that affect the application of a sloped ceiling in a seismic design category and USG recommends that the design team, consulting engineers and code officials work together to analyze these factors and determine the appropriate construction and application of lay-in acoustical sloped ceilings. Because codes continue to evolve, check with a local official prior to designing and installing a sloped ceiling system.

# Guidelines

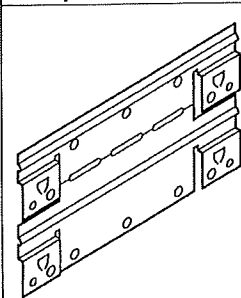
## Sloped Ceilings

- Hold-down clips should be used for ceilings with slopes  $\geq 15^\circ$ .
- At least one hold-down should be used for each panel installed on the perimeter of the higher/top end of the slope.
- USG V15 and L15 hold-down clips are acceptable for use in sloped installations.
- The main tees shall run up/down, parallel with the slope.
- Hanger wires shall be suspended vertically.
- Struts shall remain vertical and the splay wires installed at max.  $45^\circ$  to the horizontal.
- Fabricated or locally sourced wall molding can be used to match the angles of the slope.
- For installations where 7/8-in. wall molding is desired and the slope is  $> 15^\circ$ , it is recommended that USG M7 wall molding be installed with the USG adjustable wall molding bracket.
- For installations utilizing the SB2 adjustable wall molding bracket, the brackets shall be spaced max. 24 in.
- Lower/bottom end of slope should be installed as a tight side in seismic design categories D, E and F.
- Higher/top end of slope should be installed as a floating side in seismic design categories D, E and F.
- The USG MAC2 clip may be used to secure the tee ends to the wall molding and field modified to match the slope of the tees.
- For installations with fabricated molding where the slope is  $< 15^\circ$ , the USG ACM7 seismic clip may be used on the lower/bottom end of slope and shall be installed with the back flanges on the inside of the wall molding with a fastener through either top fastener hole on the wing of the clip.
- For installations with fabricated molding where the slope is  $< 15^\circ$ , the USG ACM7 seismic clip may be used on the higher/top end of slope and shall be installed with the back flanges on the inside of the wall molding with a fastener through either lower fastener hole on the wing.
- Ceilings with slopes  $> 25^\circ$  should be engineered.

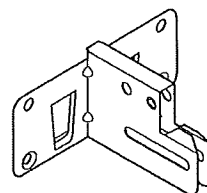
# Components

## Sloped Ceilings

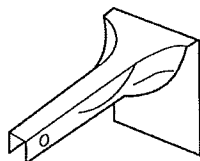
SB2 Adjustable Wall Molding Bracket



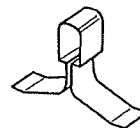
ACM7 Seismic Clip



MAC2 Clip



Panel Hold Down Clip



# Requirements

## Sloped Ceilings

### Seismic Requirements<sup>1</sup>

	Seismic Design Category C	Seismic Design Category D, E, F
Main-tee Classifications	Intermediate Duty	Heavy Duty
Perimeter vertical hanger wires not more than 8 in. from wall	Not required <sup>2</sup>	Required
Grid end/wall clearance	3/8 in.	3/4 in. (two adjacent sides)
Perimeter closure (molding) width <sup>3</sup>	7/8-in. min.	2-in. min. without seismic clip
Perimeter tee ends tied together at perimeters <sup>4</sup>	Required	Required
Horizontal restraint (splay wires or rigid bracing) within 2 in. of intersection and splayed 90° apart at 45° angles for ceiling areas > 1,000 sq. ft.	Not required	Required
Compression posts (struts) 12 ft. o.c. in both directions, starting 6 ft. from walls for ceiling areas > 1,000 sq. ft.	Not required	Required
Seismic separation joint for ceiling areas > 2,500 sq. ft.	Not required	Required

<sup>1</sup> This is only intended as a quick reference. For a complete listing of these requirements please visit [usg.com](http://usg.com) or [seismicceilings.com](http://seismicceilings.com).

Because codes continue to evolve, check with a local official prior to designing and installing a suspended ceiling system.

<sup>2</sup> When the perimeter closure angle approved for use provides min. 7/8 in [22 mm] support ledge.

<sup>3</sup> Please see ICC-ESR-1222 for more information regarding the alternative wall moldings that may be used with the USG ACM7 seismic clip.

<sup>4</sup> USG ACM7 seismic clip satisfies this requirement per ICC-ESR-1222.

### Perimeter Treatment

Lay-in Suspended Ceiling Slope	Non-Seismic Installation	Seismic Design Category C	Seismic Design Category D, E, F
< 15°	USG M7 wall molding with positive attachment on all tee ends or 7/8-in. fabricated / locally sourced wall molding with positive attachment on all tee ends	USG M7 wall molding with ACM7 seismic clip or 7/8-in. fabricated / locally sourced wall molding with ACM7 seismic clip	USG M7 wall molding with ACM7 seismic clip or 7/8-in. fabricated / locally sourced wall molding with ACM7 seismic clip
15° – 25°	SB2 adjustable wall molding bracket with positive attachment on all tee ends or 7/8-in. fabricated / locally sourced wall molding with positive attachment on all tee ends	SB2 adjustable wall molding bracket installed with USG M7 wall molding and the USG ACM7 seismic clip	SB2 adjustable wall molding bracket installed with USG M7 wall molding and the USG ACM7 seismic clip
> 25°	Fabricated / locally sourced wall molding with positive attachment on all tee ends	Engineered	Engineered



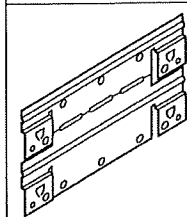
# Sloped Ceiling Perimeter Treatment

## SB2 Adjustable Wall Molding Bracket

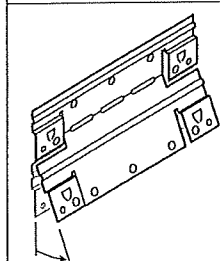
The SB2 adjustable wall molding bracket is a convenient way to secure standard wall molding at an angle to accommodate a sloped acoustical ceiling.

The brackets can be field modified to accommodate numerous slopes while utilizing standard wall molding. The SB2 adjustable wall molding bracket eliminates the need for fabricated wall molding and facilitates the use of the USG ACM7 seismic clip in sloped applications. Seismically tested and listed in PEI Evaluation Report, PER-12059.

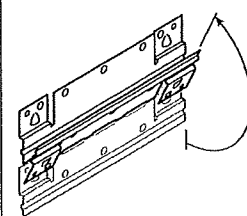
SB2 Adjustable Wall Molding Bracket



Up slope



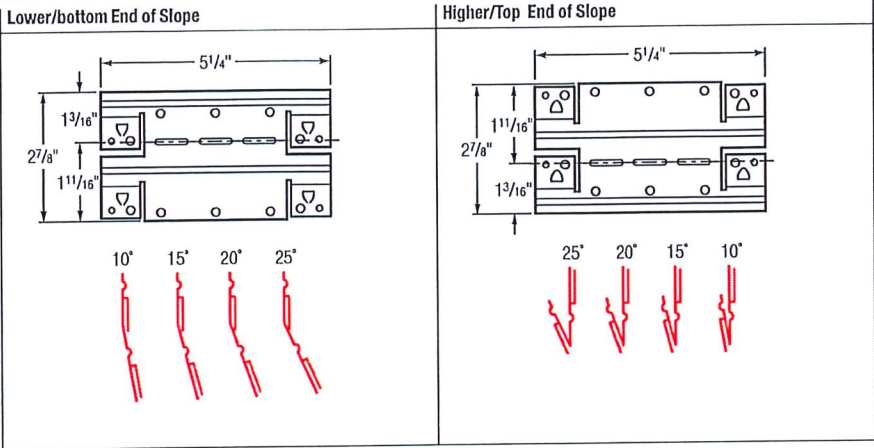
Down slope



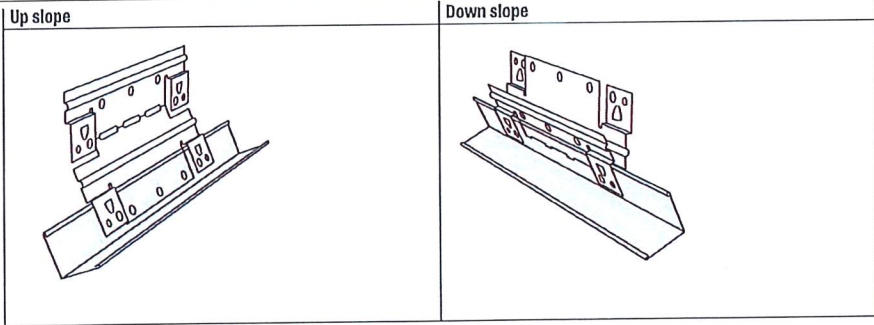
# SB2 Adjustable Wall Molding Bracket

## Installation

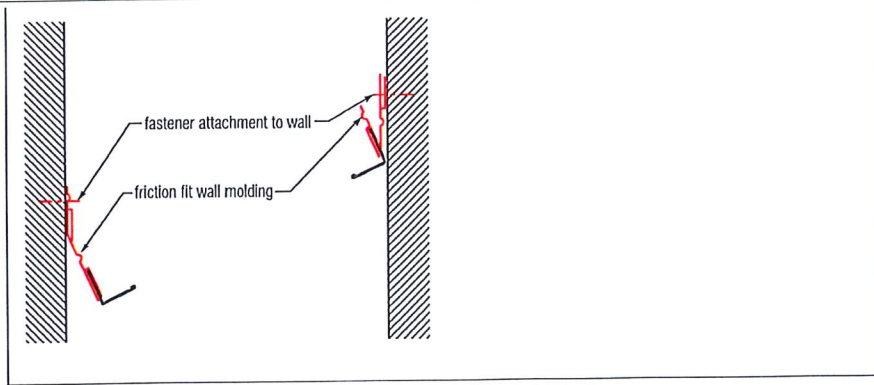
### Typical Angles



### Placement



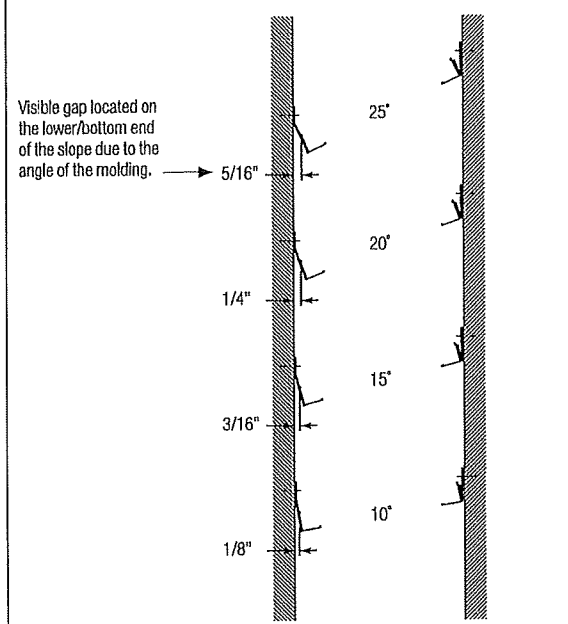
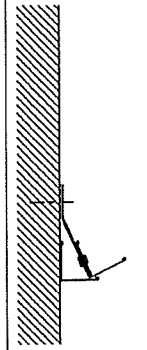
### Attachment to Wall



# SB2 Adjustable Wall Molding Bracket

## Installation

### Gaps

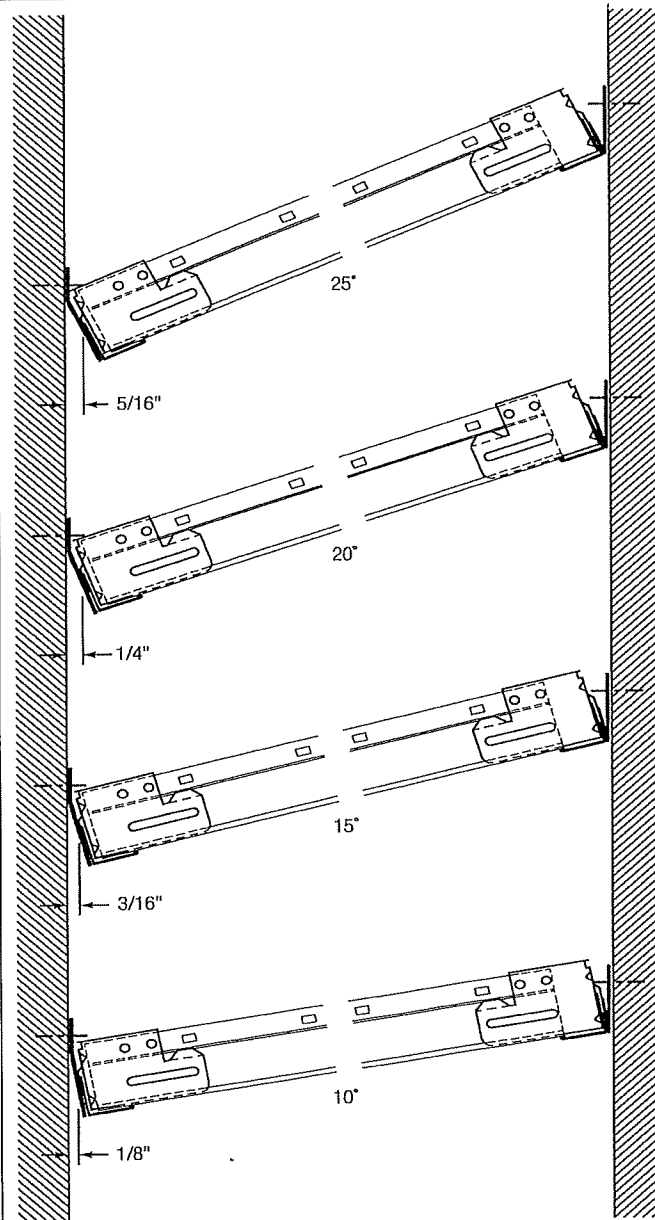
Clearance Gaps	Additional Wall Molding to Finish Gap
<p>Visible gap located on the lower/bottom end of the slope due to the angle of the molding. → 5/16"</p>  <p>25°</p> <p>20°</p> <p>15°</p> <p>10°</p> <p>5/16"</p> <p>1/4"</p> <p>3/16"</p> <p>1/8"</p>	 <p><b>Note:</b> Additional wall molding may be applied below the angled wall molding to conceal the gap created by the sloped ceiling.</p>

# SB2 Adjustable Wall Molding Bracket

## Installation

### Seismic Applications

#### Angles and gaps with ACM7 Clip



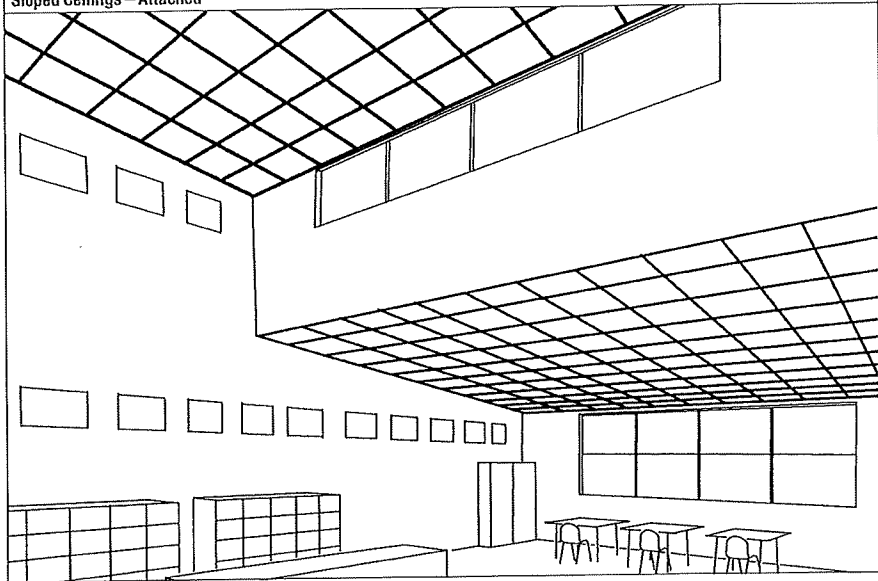
Note: 3/4" gap shown for typical seismic design categories D-F. 3/8" gap is typical for seismic design category C.

# Construction

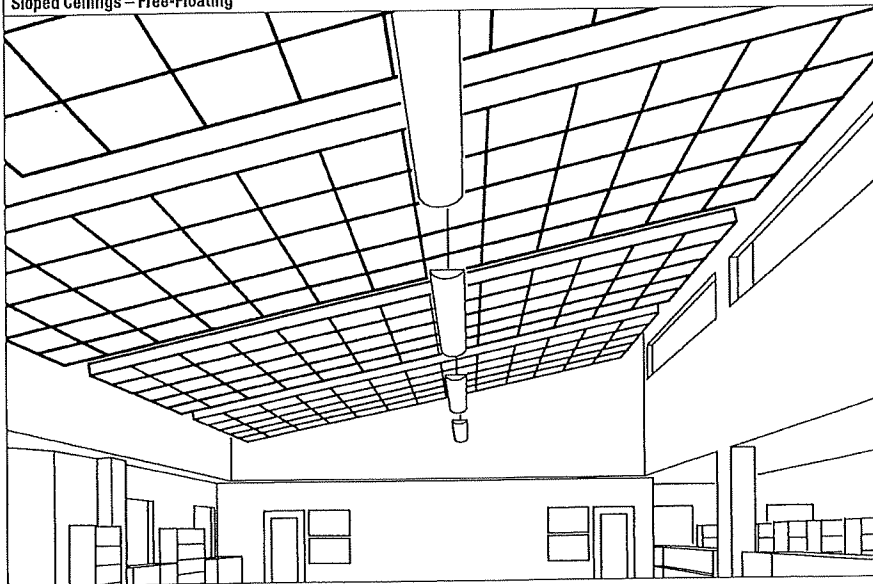
## Attached Free-Floating

Generally, sloped suspended ceilings are installed attached at the perimeter or free-floating from the perimeter.

Sloped Ceilings – Attached



Sloped Ceilings – Free-Floating





# Construction

## Seismic Non-Seismic

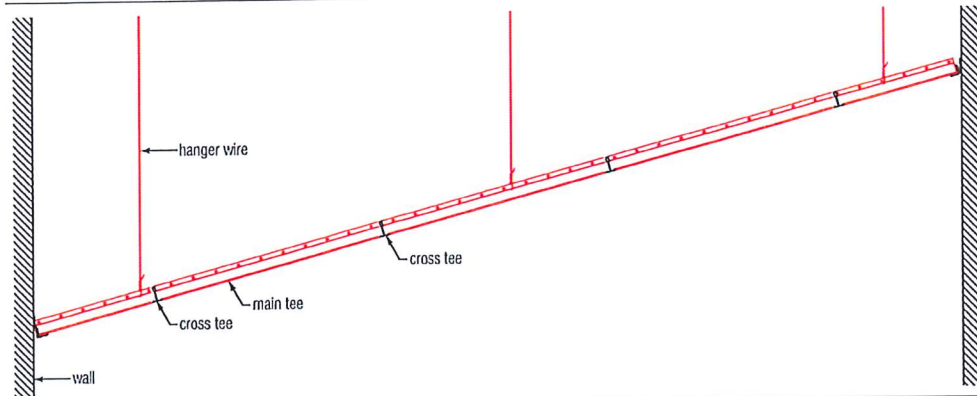
Construction Category	Non-Seismic Installation		Seismic Installation			
	Attached	Free-floating	Attached		Free-floating	
	—	—	Seismic Design Category C	Seismic Design Category D, E, F	Seismic Design Category C	Seismic Design Category D, E, F
Main-tee Classifications	Intermediate or Heavy Duty	Intermediate or Heavy Duty	Intermediate Duty	Heavy Duty	Intermediate Duty	Heavy Duty
Perimeter vertical hanger wires not more than 8 in. from wall	Not required	Not required	Not required	Required	N/A	N/A
Grid end/wall clearance	N/A	N/A	3/8 in.	3/4 in. (two adjacent sides)	N/A	N/A
Perimeter closure (molding) width	N/A	N/A	7/8-in. min.	2-in. min. without seismic clip	A perimeter trim or channel molding is typically used.	A perimeter trim or channel molding is typically used.
Perimeter tee ends tied together at perimeters*	Not required	Not required	Required	Required	Required	Required
Horizontal restraint (splay wires or rigid bracing) within 2 in. of intersection and splayed 90° apart at min. 45° angles for ceiling areas > 1,000 sq. ft.	Not required	Not required	Not required	Required	Not required	Required
Compression posts (struts) 12 ft. o.c. in both directions, starting 6 ft. from walls for ceiling areas > 1,000 sq. ft.	Not required	Not required	Not required	Required	Not required	Required
Seismic separation joint for ceiling areas > 2,500 sq. ft.	Not required	Not required	Not required	Required	Not required	Required
Hold-down clips	Not required	Not required	At least one installed for each panel located at the perimeter of the higher/top end of the slope. At least one installed for each panel in ceilings with slopes $\geq 15^\circ$ .	At least one installed for each panel located at the perimeter of the higher/top end of the slope. At least one installed for each panel in ceilings with slopes $\geq 15^\circ$ .	At least one installed for each panel located around the perimeter in ceilings with slopes $\geq 15^\circ$ .	At least one installed for each panel located around the perimeter in ceilings with slopes $\geq 15^\circ$ .

\*USG ACM7 seismic clip satisfies this requirement per ICC-ESR-1222.

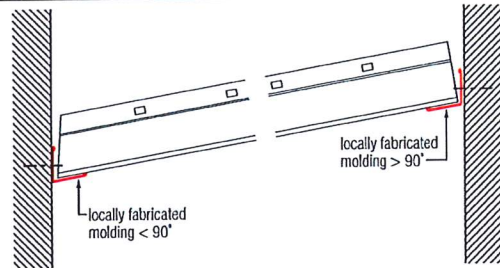
# Applications

## General

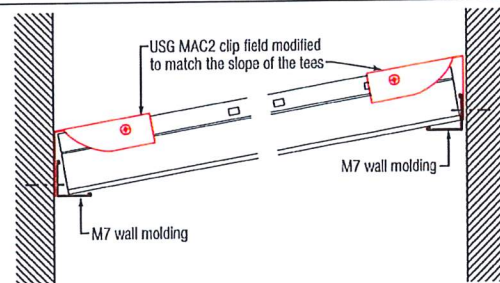
Main tees parallel with the slope and hanger wires suspended vertically.



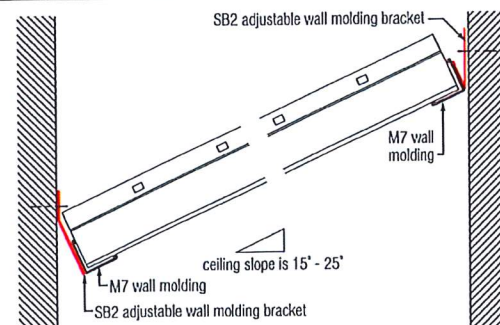
Fabricated or locally sourced molding can be used to match the angles of the slope.



The USG MAC2 clip may be used to secure the tee ends and field modified to match the slope of the tees



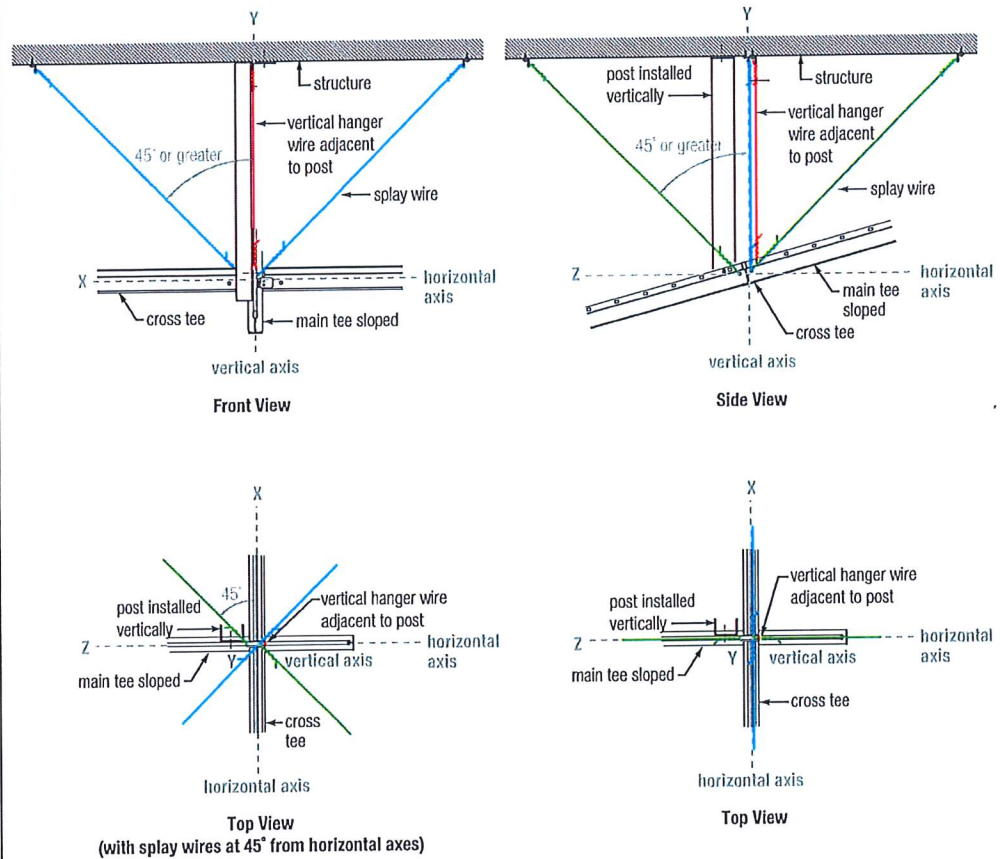
For installations where the slope is 15° - 25°, it is recommended that USG M7 molding be installed with the USG adjustable wall molding bracket.



# Applications

## Lateral Bracing

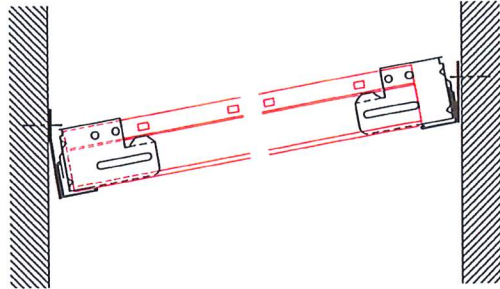
When required, struts are to remain vertical and the splay wires installed at max. 45° to the horizontal.  
Please refer to SC2497 for more information about struts.



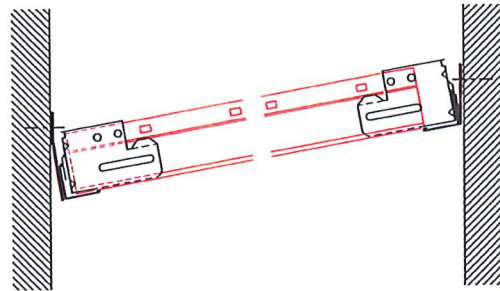
# Applications

## Seismic Design Categories

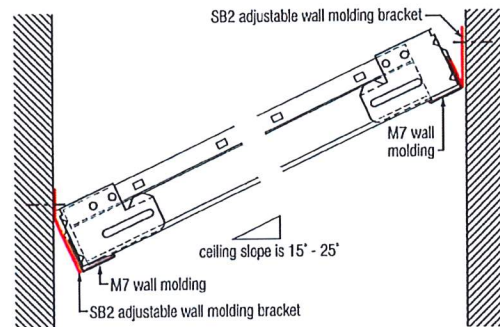
Lower/bottom end of slope should be installed as a tight side.



Higher/top end of slope should be installed as a floating side.



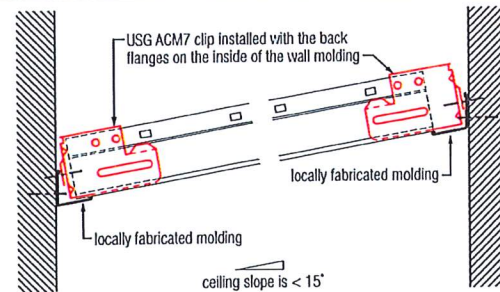
For installations where 7/8-in. wall molding is desired and the slope is  $15^{\circ}$  –  $25^{\circ}$ , it is recommended that USG M7 molding be installed with the SB2 adjustable wall molding bracket.



For installations with fabricated molding where the slope is  $< 15^{\circ}$ , the USG ACM7 seismic clip may be used on the *lower/bottom end of slope* and shall be installed with the back flanges on the inside of the wall molding with a fastener through either top fastener hole on the wing of the clip. Note a slight mouse hole or opening may be visible in this application.

### Notes:

- The USG ACM7 seismic clip is not designed to function with wall molding fabricated at an angle  $> 105^{\circ}$  or  $< 75^{\circ}$ .
- 3/4" gap shown for typical seismic design categories D-F.
- 3/8" gap is typical for seismic design category C.



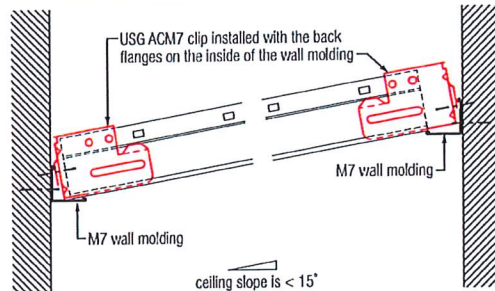


# Applications

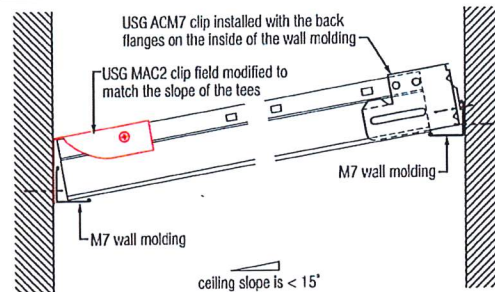
## Attachments Methods

For installations with standard wall molding where the slope is  $< 15^\circ$ , the USG ACM7 seismic clip may be used on the *higher/top end of slope* and shall be installed with the back flanges on the inside of the wall molding with a fastener through either lower fastener hole on the wing.

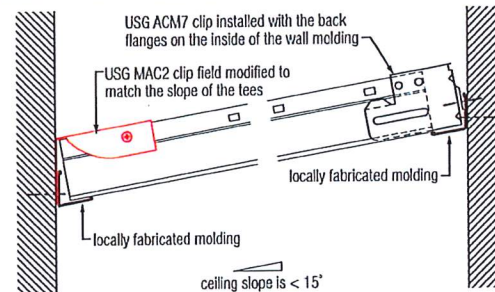
**Note:** The USG ACM7 seismic clip is not designed to function with wall molding fabricated at an angle  $> 105^\circ$  or  $< 75^\circ$ .



For installations with standard wall molding where the slope is  $< 15^\circ$ , the USG MAC2 clip may be used to secure the tee ends on the lower/bottom end of slope and field modified to match the slope of the tees.



For installations with fabricated molding where the slope is  $< 15^\circ$ , the USG MAC2 clip may be used to secure the tee ends on the lower/bottom end of slope and field modified to match the slope of the tees.



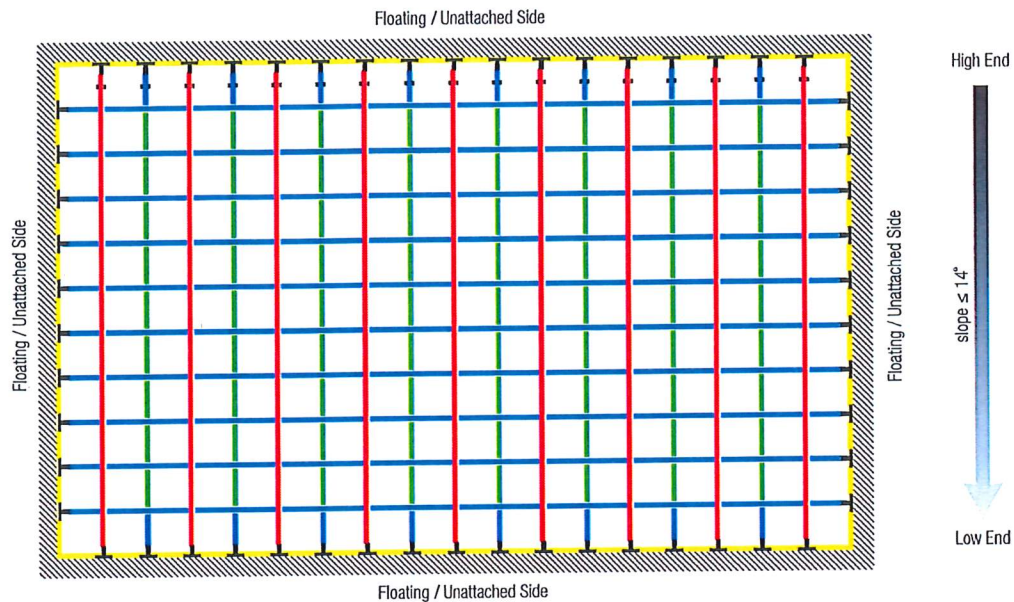
# Sloped Layouts

## Seismic Design Categories

- KEY
- Main Tee
  - Perimeter Trim
  - Panel Hold-Down Clip
  - 4 ft. Cross Tee
  - ┐ USG ACM7 Seismic Clip
  - ⋯ Stabilizer Bar
  - 2 ft. Cross Tee
  - ┘ USG ACM7 Seismic Clip or USG MAC2 Clip
  - ▨ Surrounding Wall

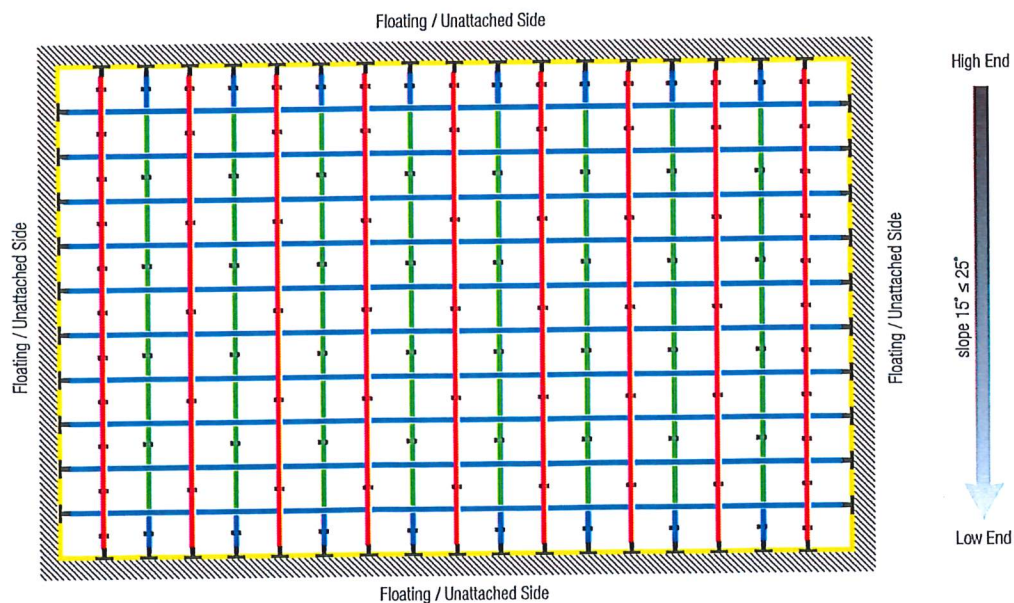
### Category C

Slope:  $\leq 14^\circ$



### Category C

Slope:  $15^\circ \leq 25^\circ$





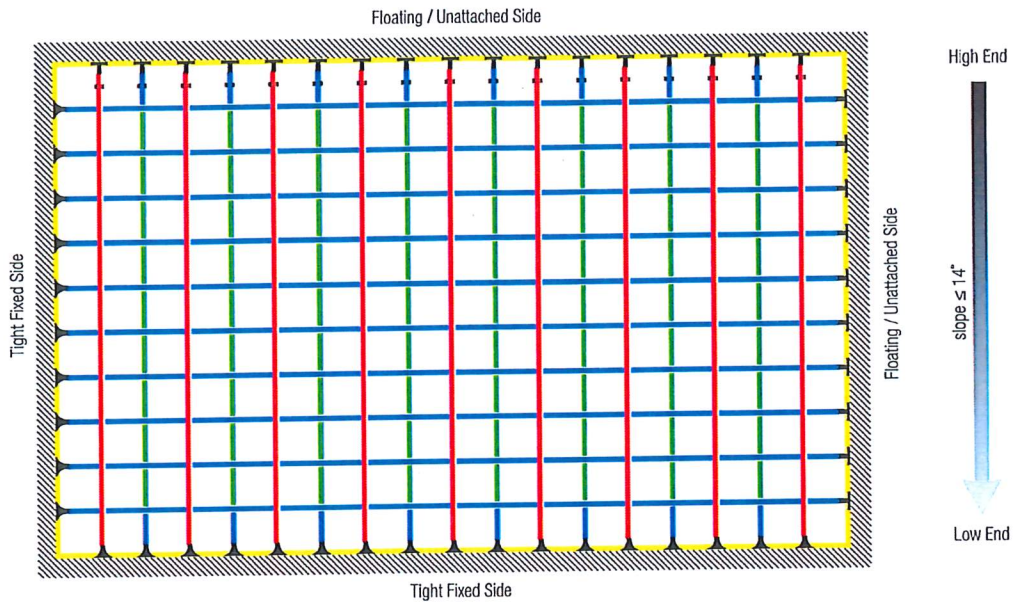
# Sloped Layouts

## Seismic Design Categories

- |     |                 |  |                      |
|-----|-----------------|--|----------------------|
| KEY | Main Tee        | Perimeter Trim                         | Panel Hold-Down Clip |
|     | 4 ft. Cross Tee | USG ACM7 Seismic Clip                  | Stabilizer Bar       |
|     | 2 ft. Cross Tee | USG ACM7 Seismic Clip or USG MAC2 Clip | Surrounding Wall     |

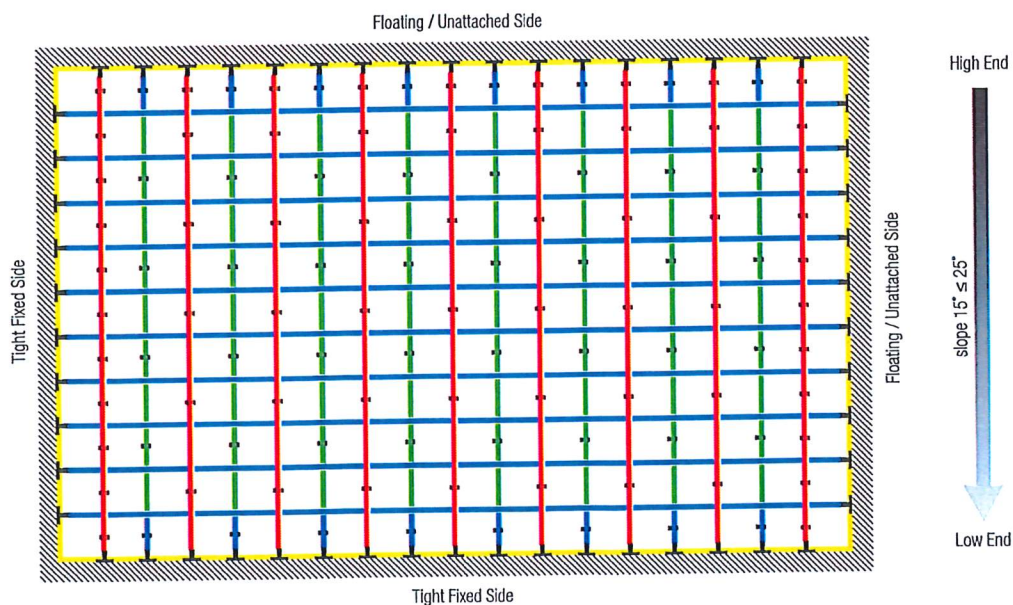
Category D, E, F

Slope:  $\leq 14^\circ$



Category D, E, F

Slope:  $15^\circ \leq 25^\circ$

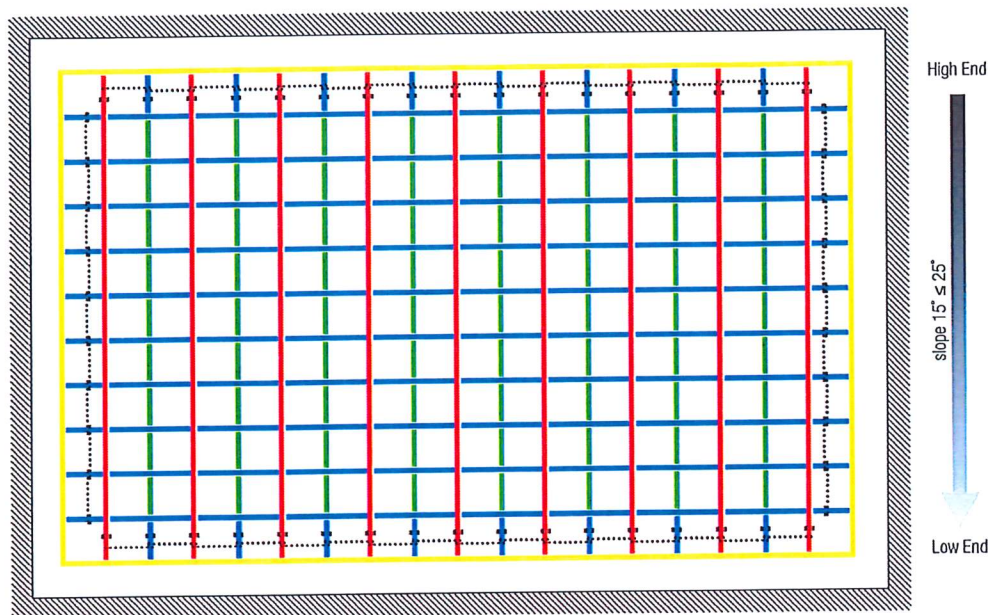


# Sloped Layouts

## Seismic Design Categories

KEY	Main Tee	Perimeter Trim	Panel Hold-Down Clip
	4 ft. Cross Tee	USG ACM7 Seismic Clip	Stabilizer Bar
	2 ft. Cross Tee	USG ACM7 Seismic Clip or USG MAC2 Clip	Surrounding Wall

Category C, D, E, F Free Floating Slope:  $15^\circ \leq 25^\circ$





# Seismic Code Reference Standards

Installation Guidelines for Suspended Ceilings				
International Building Code (IBC)	2003 IBC ↓	2006 IBC ↓	2009 IBC ↓	2012 IBC ↓
American Society of Civil Engineers (ASCE)	ASCE7-02 ↓	ASCE7-05 ↓	ASCE7-05 ↓	ASCE7-10 ↓
Ceilings Interior Systems Construction Association (CISCA) or ASTM International (ASTM)	CISCA Zones 0-2 CISCA Zones 3-4	CISCA Zones 0-2 CISCA Zones 3-4	CISCA Zones 0-2 CISCA Zones 3-4	ASTM E580

**International Building Code (IBC)** defines Seismic Design Categories A, B, C, D, E, and F.  
[www.iccsafe.org](http://www.iccsafe.org)

**ASCE/SEI 7 Minimum Design Loads for Buildings and Other Structures**  
American Society of Civil Engineers/Structural Engineer Institute (ASCE/SEI)  
[www.asce.org](http://www.asce.org)

**Guidelines for Seismic Restraint for Direct-hung Suspended Ceiling Assemblies (Zones 3-4)**  
**Recommendations for Direct-hung Acoustical Tile and Lay-in Panel Ceilings (Zones 0-2)**  
CISCA Ceilings & Interior Systems Construction Association (CISCA)  
[www.cisca.org](http://www.cisca.org)

**ASTM International E580/E580M Standard Practice for Installation of Ceiling Suspension Systems for Acoustical Tile and Lay-in Panels in Areas Subject to Earthquake Ground Motions.**  
ASTM International (formerly American Society for Testing and Materials)  
[www.astm.org](http://www.astm.org)

**Further References**      **USG Seismic Ceiling Resource Center**  
Seismic Technical Guides  
[seismicceilings.com](http://seismicceilings.com)

## CAUTION

Avoid bending the bracket back and forth too many times which will cause metal fatigue and weaken the bracket.

## Product Information

See [usg.com](http://usg.com) for the most up-to-date product information.

## Installation

Must be installed in compliance with ASTM C636, ASTM E580, CISCA, and standard industry practices. ASTM C636 and the current building code limit the installation of suspended ceilings to level applications and do not address sloped ceiling conditions. Many jurisdictions accept the installation of sloped suspended ceilings, however, some jurisdictions interpret the current standards to exclude sloped ceiling installations. Check with a local official prior to designing and installing a sloped ceiling system.

## Code Compliance

The information presented is correct to the best of our knowledge at the date of issuance. Because codes continue to evolve, check with a local official prior to designing and installing a ceiling system. Other restrictions and exemptions may apply. This is only intended as a quick reference.

## Purpose

This seismic technical guide (STG) is intended as a resource for design professionals, to promote more uniform criteria for plan review and jobsite inspection of projects. This STG indicates an acceptable method for achieving compliance with applicable codes and regulations, although other methods proposed by design professionals may be considered and adopted.

## ICG Evaluation Service, Inc., Report Compliance

Suspension systems manufactured by USG Interiors, Inc., have been reviewed and are approved by listing in ICC-ES Evaluation Report 1222. Evaluation Reports are subject to reexamination, revision and possible cancellation. Please refer to [usgdesignstudio.com](http://usgdesignstudio.com) or [usg.com](http://usg.com) for current reports.

## L.A. Research Report Compliance

Down brand suspension systems manufactured by USG Interiors, Inc., have been reviewed and are approved by listing in the following L.A. Research Report number: 25764.

## Progressive Engineering Inc. Evaluation Report Compliance

Seismically tested and listed in PEI Evaluation Report, PER-12059.

## Notice

We shall not be liable for incidental and consequential damages, directly or indirectly sustained, nor for any loss caused by application of these goods not in accordance with current printed instructions or for other than the intended use. Our liability is expressly limited to replacement of defective goods. Any claim shall be deemed waived unless made in writing to us within thirty (30) days from date it was or reasonably should have been discovered.

## Safety First!

Follow good safety/industrial hygiene practices during installation. Wear appropriate personal protective equipment. Read MSDS and literature before specification and installation.



Manufactured by  
USG Interiors, LLC  
550 West Adams Street  
Chicago, IL 60661

[usg.com](http://usg.com)  
[seismicceilings.com](http://seismicceilings.com)  
[sustainableceilings.com](http://sustainableceilings.com)  
[usgdesignstudio.com](http://usgdesignstudio.com)

The following are trademarks of USG Interiors, Inc. or a related company: USG, USG in stylized letters. LEED is a registered trademark of U.S. Green Building Council.

SC2508/rev. 6-13  
© 2013, USG Interiors, LLC  
Printed in U.S.A.



## CONSTRUCTION

"Committed to Your Success"

GENERAL BUILDING CONTRACTORS

6.1.23

### Change Proposal Request

10

HOME OFFICE:  
151 S. 18TH ST.  
SPARKS, NV 89431  
TEL: 775-356-0150  
FAX: 775-356-0247

Paul Cavin Architect, LLC  
1575 Delucchi Lane  
Reno NV 89502

re: RTAA Duty Manager Office Bldg. Remodel

Dear Paul,

reymanbrothers.com Presenting the following Change Proposal for review and approval.

Repair windows in Break Room 212 per RFI 22 response (see attached)

#### LICENCES:

NEVADA  
#12481C

CALIFORNIA  
#702410

Classic Finishes (see attached)

\$491.00

Capital Glass (see attached)

\$750.00

sub-total	\$1,241.00
OH&P @ 15%	\$124.10
<b>TOTAL ADD</b>	<b>\$1,365.10</b>

CONTRACT TIME ADJUSTED:  
NA

Sincerely,

Jace E. Callender  
Project Manager

Approved by: \_\_\_\_\_  
Paul Cavin Design

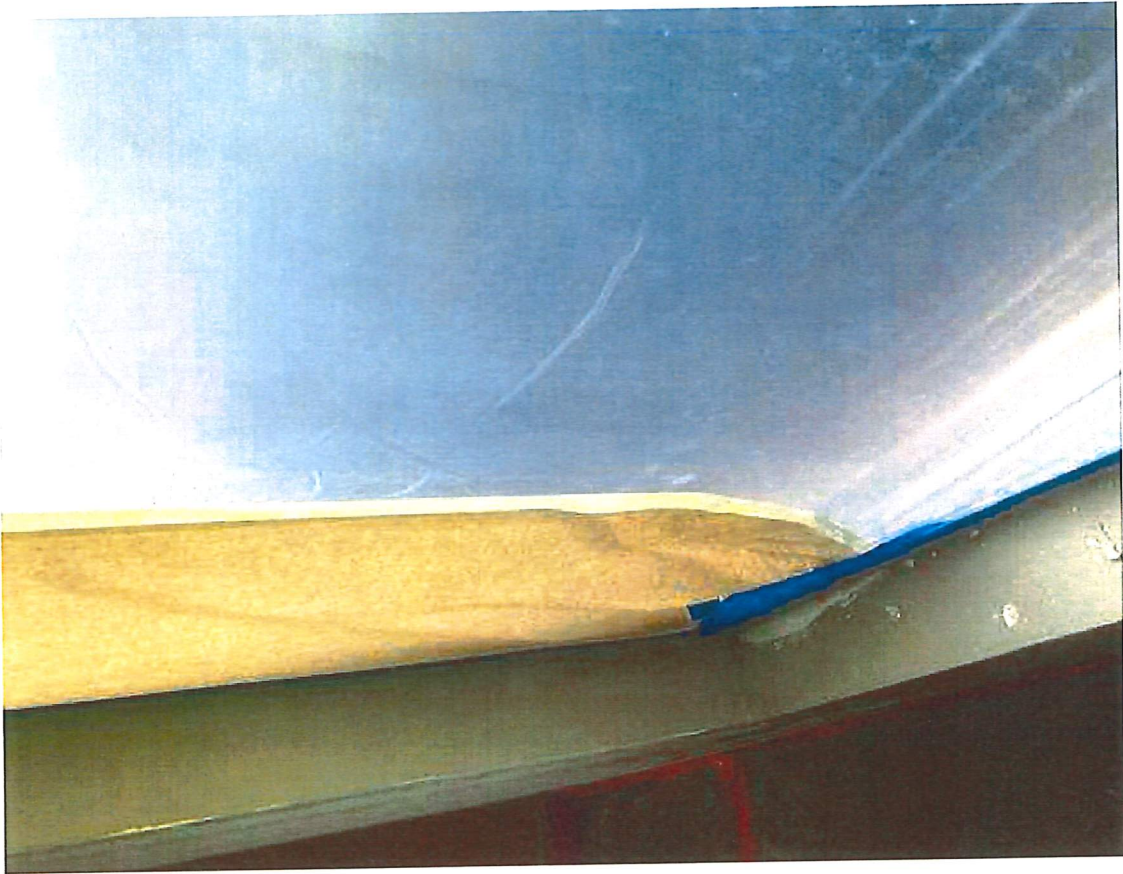
Date Approved: \_\_\_\_\_











Sent from my iPhone



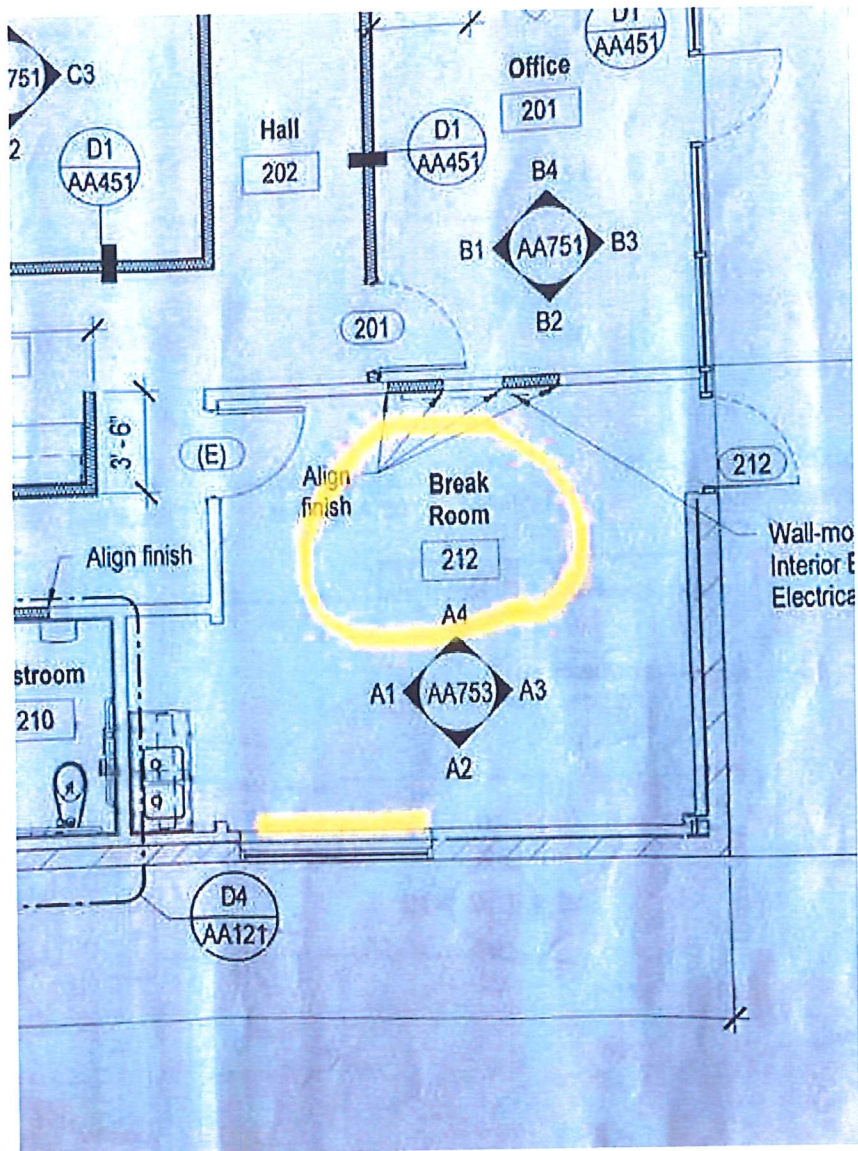
## REQUEST FOR INFORMATION

<b>PROJECT NAME:</b> RTAA Duty Manager Office Bldg remodel		<b>RESPOND BY:</b>		<b>ASAP</b>
		<b>GC'S RFI NO.</b>		22
		<b>SUB'S RFI NO.</b>		
<b>TO</b> Paul Cavin Architects LLC		<b>FROM:</b> REYMAN BROS. CONSTRUCTION TEL: 775-356-0150 FAX: 775-356-0247		
<b>RESPONSE BY:</b>				
<input type="checkbox"/> System Eng. Consultants:		<input type="checkbox"/> Mechanical		
		<input type="checkbox"/> Plumbing		
		<input type="checkbox"/> Electrical		
<input type="checkbox"/> Structural Eng. Consultants:				
<input type="checkbox"/> Other Consultants:				
<b>SUBCONTRACTOR:</b>				
<b>SUBJECT:</b> Leaking Windows				
<b>CATEGORY:</b>				
<input type="checkbox"/>	NEED FOR CLARIFICATION	<input type="checkbox"/>	OMISSION	
XXX	UNFORSEEN CONDITION	<input type="checkbox"/>	COORDINATION PROBLEM	
<input type="checkbox"/>	CONFLICT WITHIN DOCUMENTS	<input type="checkbox"/>	OTHER	
<b>SPEC. SECTION</b>		<b>PARAGRAPH NO.</b>		<b>DRAWING NO.</b>
				<b>DETAIL NO.</b>
<b>DESCRIPTION:</b> The existing windows in Break Room 212 are leaking and have caused damage, please see attached photos and advise how to proceed.				
<b>ATTACHMENTS:</b>				
<b>COST IMPACT:</b>		<b>EST.</b>	<b>TIME IMPACT:</b>	
<b>CONTRACTOR SIGNATURE:</b> <i>Jack C. Callender</i>			<b>DATE:</b> 5.24.23	
<b>RESPONSE:</b>				
<div style="border: 2px solid red; padding: 10px; background-color: #d4edda;">Please coordinate with Capital Glass on replacing missing/damaged seals and caulking on inside and outside of the system. Also coordinate with Classic Finishes in repairing and painting window surrounds once Capital Glass is finished with repairs.  Jeffrey Purcell, Paul Cavin Architect LLC, 05/26/23</div>				
<b>ATTACHMENTS:</b>				
<b>SIGNATURE:</b>			<b>DATE:</b>	



From: [Josh Jenssen](#)  
To: [Elizabeth Callender](#)  
Cc: [Darrell Linscott](#)  
Subject: Reno Tahoe Airfield Maintenance water damage  
Date: Wednesday, May 24, 2023 1:08:04 PM

---





775.470.5218 Office  
775.470.5303 Fax



Nevada Contractors  
Licenses:  
#80455 • C4  
#80454 • C17

708 N. Center Street • Reno, NV 89501

May 30, 2023

Reyman Brothers Construction  
151 S 18th St  
Sparks, NV 89431

(775) 356-0150

Re: Reno-Tahoe International Airport Base Operations and Airfield Maintenance Office Improvements.

To whom it may concern,

We are pleased to provide you with our estimate to furnish and install the following services and materials for the above-referenced project. We are signatory to our respective trade unions, fully licensed, and bondable at a rate of approximately 2.50%. Please review the following for scope and exclusions.

This estimate is based on RFI 22.

Work Included:

Change Order Request:

- Patch and Repair (1) One Window Surround in Break Room #212 with 5/8" Type X Gypsum Board, and Tape & Finish to match existing Skip Trowel textured adjacent walls.

**Total Change Order Request Estimate (Including Tax): \$491.00**

775.470.5218 Office  
775.470.5303 Fax



Nevada Contractors  
Licenses:  
#80455 • C4  
#80454 • C17

708 N. Center Street • Reno, NV 89501

Work Excluded:

- \*\*Demolition
- \*\*Wood, Plywood, Wood Blocking/Firestopping, and or wood carpentry.
- \*\*Metal, Aluminum, and/or Plastic Corner Guards.
- \*\*Access Panels and/or installation of.
- \*\*Aluminum Brake Metal, and/or any Extruded Metal Products to match Storefronts or Windows.
- \*\*Metal Flashing & Copings.
- \*\*110 & 220 3 phase power sources.
- \*\*Grouting hollow metal door frames.
- \*\*Color coding, I.D. banding sprinklers pipe painting.
- \*\*Striping and signage paint.
- \*\*Concrete floor sealers/hardeners.
- \*\*Repair and patching Classic Finishes scope of work, due to damage by others.
- \*\*Electrical/mech/plumbing fixture support wires.
- \*\*Fire rating light fixtures and electrical outlets.
- \*\*Firesafing, fire-caulking, firestopping.
- \*\*Sound batt and/or Thermal Insulation.
- \*\*Caulking/sealants.
- \*\*Classic Finishes standard exclusions consist of the following:
  - ##Overtime (Over 40 Hours/Week); Weekends, Nightwork, or Shiftwork.
  - ##Temporary heat or weather protection.
  - ##Lighting, task, and area type.
  - ##All water requirements, minimum at 50 psi.
  - ##Testing, permits, bonds, fees, grade setting.
  - ##Temporary facilities, barriers, enclosures, and waste management.
  - ## No shop drawings, design build, as-built drawings, engineering, or calculations.

All work proposed must be ready for continuous progress and in-sequence work. Costs incurred by Classic Finishes, due to the delay by others shall be billed to General Contractor accordingly.

Should Classic Finishes be awarded this project, our bid Proposal in its entirety shall be included as an exhibit to the contract, as it is specific to our scope. **Our Scope is limited to the above referenced scope inclusions and exclusions, no other assumptions have been made.**

We thank you for the opportunity to submit an estimate for the above upcoming project. If you have any questions or comments, please feel free to call.

Classic Finishes, LLC Nevada Licenses:

- 1) #80455-C4: Limit: Unlimited 2) #80454-C17: Limit: Unlimited

Respectfully submitted,

Lorenzo Taormina  
Estimating

copy: rg

Quoted prices valid for 30 days unless otherwise noted. All material is guaranteed to be as specified. All work to be completed in a workmanlike manner, according to standard practices. Any alteration or deviation from above specifications involving extra cost will be executed only upon written orders and will become an extra charge over and above the proposal. All agreements contingent upon strikes, accidents, or delays beyond our control. Our workers are fully covered by worker's compensation Insurance. Issuance of subcontract deems acceptance to all conditions set forth.

Exterior Panel Systems • Commercial • Lath • Plaster  
Acoustic Ceilings • Drywall • Paint • Wall Covering

## Darrell Linscott

---

**From:** Jon Zalaznik <JonZ@capitalglassnv.com>  
**Sent:** Thursday, June 1, 2023 8:52 AM  
**To:** Darrell Linscott  
**Cc:** Jace Callender; Keven Olsen; Elizabeth Callender  
**Subject:** RE: RTAA DUTY MANAGER OFFICE - RFI 22 LEAKING WINDOWS

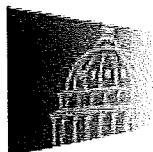
Darrell,

Looks like we'd need to reseal the frame from the exterior.

Plug in \$750.00

Thank you,

Jon Zalaznik  
Project Manager



CapitalGlass

11331 S. Virginia St. #3  
Reno, NV 89511

Contact #'s: Carson City: 775.883.6401, Reno: 775.324.6688 Ext. 156

[www.CapitalGlassNV.com](http://www.CapitalGlassNV.com)

Lic. #'s: NV: 10139, CA: 811436, AZ: 265620

(PLEASE REPLY ALL WHEN RESPONDING)

**From:** Darrell Linscott <dlinscott@Reymanbrothers.com>  
**Sent:** Thursday, June 1, 2023 8:34 AM  
**To:** Jon Zalaznik <JonZ@capitalglassnv.com>  
**Cc:** Jace Callender <jcallender@Reymanbrothers.com>; Keven Olsen <kolsen@Reymanbrothers.com>; Elizabeth Callender <ecallender@Reymanbrothers.com>  
**Subject:** FW: RTAA DUTY MANAGER OFFICE - RFI 22 LEAKING WINDOWS

Jon,  
Just a friendly reminder. Pricing for this is due today by Noon.  
Thanks,

Darrell Linscott  
Project Manager  
**REYMAN BROTHERS CONSTRUCTION INC**  
*"Committed to Your Success"*  
151 S 18<sup>th</sup> Street  
Sparks, NV 89431

# RENO-TAHOE AIRPORT AUTHORITY CONTINGENCY CHANGE ORDER



CCO No.

Contractor:

Project:

Solicitation Number:

## Summary of Change and List of Attachments:

The contract currently has a previous Change Order No. 1 for contract days extension at no cost. Change Order No. 1 should have been documented as "Contingency Change Order No. 1" (CCO No. 1). This Contingency Change Order No. 2 (CCO No. 2) will reflect the correction and document as such. CCO No. 2 is also for the items described below. PCO cover sheets are attached.

PCO No. 1 - Removal of Fire Smoke Dampers (\$1,069.00)  
 PCO No. 2 - Remove and Install Additional Fire Dampers (\$3,684.00)  
 PCO No. 3 - Duct Detectors (\$1,133.00)

## Contingency Change Order Summary:

Contract Contingency Total: \$

Total Previously Approved: \$

Total Change this Authorization: \$  ADD/DEDUCT

Remaining Contingency Balance: \$

## Change in Contract Times:

Original Duration:

Previous Authorization:

This Authorization:  ADD/DEDUCT

Revised Contract Time:

## Contract Summary:

Original Contract: \$


Total Previously Approved CO's \$

Total Previously Approved CCO's \$

Contract Sum Prior to this CCO \$

Total Change this Authorization: \$

New Contract Sum: \$

Contractor Signature  Date:

Gardner Engineering, Robert Gardner

Project Mgr Signature  Date:

RTAA Project Mgr: Amanda Twitchell

Construction Mgr Signature  Date:

Atkins, Construction Manager: Kara Bymers

Manager Signature \_\_\_\_\_ Date: \_\_\_\_\_

RTAA Mgr Engineering & Construction: Jackie Borman





Member of the SNC-Lavalin Group

Atkins North America, Inc.  
10509 Professional Circle, Suite 103  
Reno, NV 89521-8991  
Telephone: +1.775.828.1622  
Fax: +1.775.851.1687  
[www.atkinsglobal.com/northamerica](http://www.atkinsglobal.com/northamerica)

April 14, 2023

Mrs. Haley Webbert  
Project Engineer  
Gardner Engineering and  
Mechanical Services  
270 East Parr Boulevard  
Reno, NV 89512

**Subject: MZ-3 Replacement Project  
Reno-Tahoe International Airport  
Potential Change Order No. 01  
Removal of Fire Smoke Dampers**

Dear Mrs. Webbert:

This Potential Change Order (PCO) sets forth the terms and conditions to provide labor, equipment, material and full compensation to remove four (4) fire smoke dampers that have been previously installed.

**The following adjustment shall be made to the Schedule of Values:**

PCO 01-01, Removal of Fire Smoke Dampers

• One Lump Sum price \$1,069.00

The total amount approved for this change is **\$1,069.00**. Gardner Engineering and Mechanical Services has not asked for and will not receive any days added to the contract duration for this change.

If the above terms and conditions are acceptable, please indicate by signing below and returning the original letter to this office. Upon receiving all approvals, the necessary change order documentation will be processed. A fully executed copy of this PCO will be forwarded to Gardner Engineering and Mechanical Services. If you have any questions, please call me at (775) 828-1622.

Sincerely,

Kara Bymers  
Senior Construction Manager

Haley Webbert, Project Engineer  
Gardner Engineering and Mechanical Services

Concur:

Amanda Twitchell, Senior Project Manager  
Reno-Tahoe Airport Authority

Digitally signed by Jackie Borman, PE  
DN: cn=Jackie Borman, PE, o=Manager of  
Engineering and Construction, ou=Reno-  
Tahoe Airport Authority,  
email=jborman@renoairport.com, c=US  
Date: 2023.04.21 16:22:08 -0700

Jackie Borman, Manager of  
Construction Reno-Tahoe Airport Authority

Enclosure: Gardner Engineering and Mechanical Services' Letter dated March 30, 2023

File: 40.08.08



Member of the SNC-Lavalin Group

Atkins North America, Inc.  
10509 Professional Circle, Suite 103  
Reno, NV 89521-8991  
Telephone: +1.775.828.1622  
Fax: +1.775.851.1687  
[www.atkinsglobal.com/northamerica](http://www.atkinsglobal.com/northamerica)

April 27, 2023

Ms. Haley Webbert  
Project Engineer  
Gardner Engineering and  
Mechanical Services  
270 East Parr Boulevard  
Reno, NV 89512

**Subject: MZ-3 Replacement Project  
Reno-Tahoe International Airport  
Potential Change Order No. 02  
Remove and Install Additional Fire Dampers**

Dear Mrs. Webbert:

This Potential Change Order (PCO) sets forth the terms and conditions to provide labor, equipment, material and full compensation to remove two (2) existing 10-inch round supply air fire dampers; patch existing penetrations; and install one (1) 12-inch round supply and one (1) 12-inch round return air fire damper.

**The following adjustment shall be made to the Schedule of Values:**

PCO 02-01, Remove and Install Additional Fire Dampers

- One Lump Sum price \$3,684.00

The total amount approved for this change is **\$3,684.00**. Gardner Engineering and Mechanical Services has not asked for and will not receive any days added to the contract duration for this change.

If the above terms and conditions are acceptable, please indicate by signing below and returning the original letter to this office. Upon receiving all approvals, the necessary change order documentation will be processed. A fully executed copy of this PCO will be forwarded to Gardner Engineering and Mechanical Services. If you have any questions, please call me at (775) 828-1622.

Sincerely,

Kara Bymers  
Senior Construction Manager

*Haley Webbert 4/27/2023*

Haley Webbert, Project Engineer  
Gardner Engineering and Mechanical Services

Concur:

Amanda Twitchell, Senior Project Manager  
Reno-Tahoe Airport Authority

Jackie Borman, Manager of Construction  
Reno-Tahoe Airport Authority

Digitally signed by Jackie Borman, PE  
DN: cn=Jackie Borman, PE, o=Manager  
of Engineering and Construction,  
ou=Reno-Tahoe Airport Authority,  
email=jborman@renoairport.com, c=US  
Date: 2023.04.27 18:32:21 -0700

Enclosure: Gardner Engineering and Mechanical Services' Letter dated April 18, 2023

File: 40.08.08



Member of the SNC-Lavalin Group

Atkins North America, Inc.  
10509 Professional Circle, Suite 103  
Reno, NV 89521-8991  
Telephone: +1.775.828.1622  
Fax: +1.775.851.1687  
[www.atkinsglobal.com/northamerica](http://www.atkinsglobal.com/northamerica)

May 25, 2023

Ms. Haley Webbert  
Project Engineer  
Gardner Engineering and  
Mechanical Services  
270 East Parr Boulevard  
Reno, NV 89512

**Subject: MZ-3 Replacement Project  
Reno-Tahoe International Airport  
Potential Change Order No. 03  
Duct Detectors**

Dear Ms. Webbert:

This Potential Change Order (PCO) sets forth the terms and conditions to provide labor, equipment, material and full compensation for the additional work needed to trace and safe off the MZ-3 duct detectors per the enclosed response to Request for Information (RFI) No. 6.

**The following adjustment shall be made to the Schedule of Values:**

PCO 03-01, Duct Connectors	
• One Lump Sum price	\$1,133.00

The total amount approved for this change is **\$1,133.00**. Gardner Engineering and Mechanical Services has not asked for and will not receive any days added to the contract duration for this change.

If the above terms and conditions are acceptable, please indicate by signing below and returning the original letter to this office. Upon receiving all approvals, the necessary change order documentation will be processed. A fully executed copy of this PCO will be forwarded to Gardner Engineering and Mechanical Services. If you have any questions, please call me at (775) 828-1622.

Sincerely,

Kara Bymers  
Senior Construction Manager

Haley Webbert, Project Engineer  
Gardner Engineering and Mechanical Services

Concur:

Amanda Twitchell, Senior Project Manager  
Reno-Tahoe Airport Authority

Jackie Borman, Manager of Construction  
Reno-Tahoe Airport Authority

Digitally signed by Jackie Borman  
DN: C=US, E=jborman@renoairport.com,  
O=Manager of Engineering &  
Construction, OU=Reno-Tahoe Airport  
Authority, CN=Jackie Borman  
Date: 2023.06.01 14:59:50-0700

Enclosures: 1. Gardner Engineering and Mechanical Services' Letter dated May 18, 2023  
2. Ainsworth Associates Mechanical Engineers' response to RFI No. 6

File: 40.08.08

# RENO-TAHOE AIRPORT AUTHORITY CONTINGENCY CHANGE ORDER



CCO No.

Contractor:

Project:

Solicitation Number:

## Summary of Change and List of Attachments:

Provide labor, equipment, material and full compensation to replace installed 3-way toggle switches with two (2) VIVE Maestro Switches 1-Line and 1-Load to accomplish the 3-way function. The switches will communicate wirelessly to (2) newly installed ceiling mounted occupancy sensors per the enclosed letter dated June 19, 2023.

## Contingency Change Order Summary:

Contract Contingency Total: \$

Total Previously Approved: \$

Total Change this Authorization: \$  ADD/DEDUCT

Remaining Contingency Balance: \$

## Change in Contract Times:

Original Duration:

Previous Authorization:

This Authorization:  ADD/DEDUCT

Revised Contract Time:

## Contract Summary:

Original Contract: \$

Total Previously Approved CO's \$

Total Previously Approved CCO's \$

Contract Sum Prior to this CCO \$

Total Change this Authorization: \$


New Contract Sum: \$

Contractor Signature  Date:

Gardner Engineering, Robert Gardner

Project Mgr Signature  Date:

RTAA Project Mgr: Amanda Twitchell

Construction Mgr Signature  Date:

Atkins, Construction Manager: Kara Bymers

Manager Signature  Date:

RTAA Mgr Engineering & Construction: Jackie Borman

Gary Probert, Chief of Planning & Infrastructure, for Jackie Borman



June 19, 2023

Reno-Tahoe Airport Authority  
Box 12490  
Reno, NV 89510

Attention Amanda Twitchell  
Senior Project Manager

Reference MZ-3 Replacement Project – Reno-Tahoe Airport  
Solicitation #22/23-14  
GEI Job # 6636 – CE#6 – Revision 2

Subject Dimmable Switches

Dear Amanda,

The costs for CE #6 have been adjusted to include the additional lighting work in the hallway. This includes replacing the existing 3-way toggle switches in the corridor with (2) VIVE Maestro Switches 1-Line & 1-Load to accomplish the 3-way function. The switches will communicate wirelessly to (2) newly installed ceiling mounted occupancy sensors. See the attached spreadsheet for an explanation of all associated costs.

**Total amount for this scope of work.....\$3,533.00**  
**(Three Thousand Five Hundred Thirty-Three Dollars)**

Please do not hesitate to call us if you have any questions or need any additional information.

*Haley Webbert*

Project Engineer  
Office (775) 329-4133  
hwebbert@gardnereng.com

# GARDNER ENGINEERING, INC.

**Project Name:****Change Estimate #**

Date:

**MZ-3 Replacement Project - Reno-Tahoe International Airport**

## CE#6 Dimmable Switches - Revision 2

**June 19, 2023**

[illegible]



# NELSON ELECTRIC COMPANY, INC.

1410 FREEPORT AVENUE  
SPARKS, NEVADA 89431

COR # 3 Revised

(775) 358-0643 FAX (775) 358-0674  
NV LIC. #016697 CA LIC. #322768

PROPOSAL SUBMITTED TO: Gardner Engineering, Inc.		PHONE (775) 329-4133	DATE June 16, 2023
STREET 270 E. Parr Blvd.		JOB NAME RTIA-MZ3-Replacement	
CITY, STATE AND ZIP CODE Reno, NV 89502		JOB LOCATION: RTIA-MZ-3	
ARCHITECT RTIA/Ainsworth	DATE OF PLANS 9-12-22	ATTN: Danny/Haley	FAX PHONE N/A

This COR represents the costs associated with the labor & material which will be required to replace the existing Non-Dim Switching with Dimmable Switching as it pertains to the RTIA-MZ-3 Office Remodel + add ON/OFF 3-Way Occupancy Switching to the East Remodeled Corridor.

## Pricing Reflects the following:

- 1). LOTO as required.
- 2). Remove Qty. (8) Non-Dim Occupancy Switches & (2) 3-Way Switches.
- 3). Supply & Install Qty. (8) Dimmable 0-10V Occupancy Switches.
- 4). Supply & Install Qty. (2) VIVE Wall Switches & Qty. (2) Wireless Ceiling Occ Sensors.
- 4). Supply & Install wiring & Terminations to complete.
- 5). Clean up Ceiling Tile debris as needed.
- 6). Work to be perform M-F 7AM-3:30PM

Note- The #12 wire will be used to replace the #12 wire we'll be using to pull in the added Dimming conductors.

## Attachments:

Itemized Back-Up (3-pages)

## EXCLUDES:

1. Same as contract docs.
2. Overtime/Shift & Weekend Work

We propose herby to furnish material and labor, complete in accordance with the above specifications for the sum of:

**Two-Thousand Seven-Hundred Eighty-Two & 00/100 \$2,782.00**

## Authorized Signature:

A 1.5% (18% Annually) Finance Charge will be charged on all Accounts Past 30 days.

This proposal may be withdrawn if not accepted within Thirty (30) days.

All material is guaranteed to be as specified. All work to be completed in a workmanlike manner according to standard practices. Any alteration or deviation from above specifications involving extra costs will be executed only upon written orders, and will become an extra charge over and above the estimate. All agreements contingent upon strikes, accidents or delays beyond our control. Owner to carry fire, tornado and other necessary insurance. Our workers are fully covered by Workmen's Compensation Insurance. All materials will remain the property of Nelson Electric Co., Inc. until all invoices pertaining to this job are paid in full. Right of access and removal is granted to Nelson Electric Co., Inc. in the event of non-payment under the terms of this contract. The customer agrees to pay all interest and any costs incurred in the collection of this debt

**RTIA- MZ3 Replacement Project**  
**Totals (Detailed) - Bid Summary: Default**

Job Number 1111123913  
 Bid Date 11/11/2022  
 Bid Time 12:10:00 PM  
 Square Footage 0

**Material Totals**

<u>Division</u>	<u>Total</u>	<u>Factor</u>	<u>Extended</u>
COR #3	\$259.17	0.00 %	\$259.17
<b>Total</b>	<b>\$259.17</b>		<b>\$259.17</b>

**Quoted Material**

T0013 - Vive Maestro Wall Switch Line	1.00	\$0.00	\$0.00
T0014 - Vive Maestro Wall Switch Load	1.00	0.00	0.00
T0015 - Vive Maestro Wireless Ceiling Occupancy Sensor	2.00	0.00	0.00
<b>1 - Lighting (Supplier #1)</b>			<b>293.00</b>

**Total** **\$293.00**

**Sales Tax (8.27%)** **\$45.66**

**Total Material** **\$597.83**

**Labor Hours**

<u>Division</u>	<u>Total</u>	<u>Factor</u>	<u>Extended</u>
COR #3	21.48	0.00 %	21.48
<b>Total</b>	<b>21.48</b>		<b>21.48</b>

**Labor Rates**

<u>Category</u>	<u>% of Total</u>	<u>Hours</u>	<u>Base Rate</u>	<u>Burden \$</u>	<u>Burden %</u>	<u>Extended</u>
Journeyman	100.000	21.480	\$89.93	0.00	0.000	\$1,931.70
<b>Total</b>	<b>100.000</b>	<b>21.480</b>	<b>x \$89.93 (average of labor rate w/burden) =</b>			<b>\$1,931.70</b>

0

<u>Description</u>	<u>Hours</u>	<u>Rate</u>	<u>Factor</u>	<u>Extended</u>
--------------------	--------------	-------------	---------------	-----------------

**Total** **\$0.00**

**Total Labor (21.48 hours)** **\$1,931.70**

**Job Subtotal (Prime Cost)** **\$2,529.53**

**Overhead (10.00%)** **252.95**



RTIA- MZ3 Replacement Project  
Totals (Detailed) - Bid Summary: Default

Profit (0.00%)	0.00
Job Total	\$2,782.48
Actual Bid Price	\$2,782.00
Material to Direct Labor ratio: 0.24	
Prime Cost per square foot	\$0.00
Job Total per square foot	\$0.00
Actual Bid Price per square ft	\$0.00
Labor cost per square foot	\$0.00
Labor hours per square foot	0.00
Gross Profit %	
Gross Profit \$	
Net Profit %	

## RTIA- MZ3 Replacement Project

Job Number: 1111123913

## Extension By Breakdown

Item #	Description	Quantity	Price	U	Ext Price	Labor Hr	U	Ext Lab Hr
— COR #3 —								
2596	THHN #12 Black Solid CU 500S/R	340	148.00	M	50.32	4.00	M	1.36
4055	#14-12-10 Wire Termination Labor	6	0.30	E	1.80	0.08	E	0.48
4287	2G Plastic Dupl Rcpt & Switch Plate	2	64.01	C	1.28	0.07	E	0.14
7375	Yellow Wire Nut	6	0.15	E	0.90	2.00	C	0.12
11589	Demo 3-way Toggle Switch	2	1.00	E	2.00	0.10	E	0.20
24578166	#16 TFFN Stranded Black Copper Wire	700	92.50	M	64.75	4.00	M	2.80
T0005	Apply & Remove Lock Out Tag Out Existing Circuitry	1	5.00	E	5.00	1.00	E	1.00
T0006	Remove Non-Dimmable Occupancy Sensor Switch	8	1.00	E	8.00	0.20	E	1.60
T0007	0-10 Volt Dimming Occupancy Sensor	8	0.00	E	0.00	0.35	E	2.80
T0008	De-Term #12	36	0.02	E	0.72	0.03	E	1.08
T0009	Terminate #16	70	0.02	E	1.40	0.04	E	2.80
T0010	Remove & Replace 2'X2' Ceiling Tiles	20	5.00	E	100.00	0.10	E	2.00
T0011	Layout	1	5.00	E	5.00	1.00	E	1.00
T0012	Remove & Re-Install Fixture Belly Pan/ Driver Access	18	1.00	E	18.00	0.15	E	2.70
T0013	Vive Maestro Wall Switch Line	1	QUOTE	1	0.00	0.35	E	0.35
T0014	Vive Maestro Wall Switch Load	1	QUOTE	1	0.00	0.35	E	0.35
T0015	Vive Maestro Wireless Ceiling Occupancy Sensor	2	QUOTE	1	0.00	0.35	E	0.70
— COR #3 Total —					259.17			21.48
Job Total					259.17			21.48

**CHANGE  
ORDER****Distribution to:**

RTAA PURCHASING	<input checked="" type="checkbox"/>
PM	<input checked="" type="checkbox"/>
CM	<input checked="" type="checkbox"/>
ENGINEER	<input checked="" type="checkbox"/>
CONTRACTOR	<input checked="" type="checkbox"/>
FAA	<input type="checkbox"/>

**Reno-Tahoe Airport Authority**

Reno-Tahoe International Airport  
Reno-Stead Airport  
Box 12490  
Reno, NV 89510



Project: Ticketing Hall Expansion Project, GMP No. 2

Solicitation #: RFP #21/22-03

To: McCarthy Building Companies, Inc.  
2580 St. Rose Pkwy, Ste. 200  
Henderson, NV 89074

Change Order Number 03

Change Order Initiation Date: June 15, 2023

AIP No. N/A

Original Contract Date: September 8, 2022

**You are directed to make the following changes in the Contract:**

**Decrease Schedule of Values Line Item No. 34 - Owners Contingency by \$10,000.00. Funds to be transferred to RTAA soft costs for payment of items outside of the contract. (\$10,000.00)**

**All other terms, conditions, and requirements not modified herein remain unchanged.**

Not valid until signed by ALL parties. Execution of this Change Order by both Owner and Contractor constitutes a binding agreement and serves as a full accord and satisfaction of any claim, demand, lien, stop notice or further request for compensation, past or present, known or unknown, and/or time extension arising out of or by virtue of the work described above in the Change Order. Contractor's signature indicates agreement herewith, including any adjustments in the Contract Sum or Contract Time.

The Original Contract Sum was.....	\$25,678,143.00
Net Changes by Previously Authorized Change Orders .....	\$213,828.68
Net Changes by Previously Authorized Contingency Change Orders	\$0.00
The Revised Contract Sum Prior to this Change Order was .....	\$25,891,971.68
The Contract Sum will be <b>decreased</b> by this Change Order.	(\$10,000.00)
The new Contract Sum, including this Change Order will be .....	\$25,881,971.68

The Contract Completion date prior to this Change Order is July 3, 2024.

The Contract Time will not change due to this change order.

Authorized By:

**Atkins North America**

Owner's Representative  
10509 Professional Cir. Ste 103  
Reno, NV 89521

*Kara Bymers*

By: Kara Bymers

06/15/2023

Date

**RS&H**

Architect  
369 Pine Street, Suite 610  
San Francisco, California 94104

*Geoff Chevlin*

By: Geoff Chevlin

Date

**McCarthy Building  
Companies Inc.**

Contractor  
2580 St. Rose Pkwy, Ste. 200  
Henderson, NV 89074

*Ross Edwards*

By: Ross Edwards

Date

**Reno-Tahoe Airport Authority**

Owner  
P.O. Box 12490  
Reno, NV 89510

By: Jackie Borman

Date

# Administrative Report

**Date:** July 11, 2023  
**To:** All Board Members  
**From:** Daren Griffin, President/CEO  
**Subject:** Financial Reporting Package – May 2023

## EXECUTIVE SUMMARY

Attached is the Financial Reporting Package for the eleventh-month period ending May 31, 2023, of the Fiscal Year (FY) 2022-23. The package includes a high-level summary of total revenue and expenses, followed by a more detailed discussion of key metrics. A summary of financial operating results in comparison to the budget forecast is as follows:

	YEAR TO DATE as of May 31, 2023 (In Thousands)						
	Actual Results				91.7% Of Fiscal Year		
	CURRENT YEAR	PRIOR YEAR	VARIANCE		Y-T-D BUDGET	VARIANCE	
			\$	%		\$	%
<b>Operating Revenue</b>							
Airline	\$ 17,849	\$ 16,932	\$ 917	5.4%	\$ 19,223	\$ (1,374)	-7.1%
Non-Airline	42,648	38,235	4,413	11.5%	43,167	(519)	-1.2%
<b>Total Operating Revenue</b>	<b>60,497</b>	<b>55,167</b>	<b>5,330</b>	<b>9.7%</b>	<b>62,390</b>	<b>(1,893)</b>	<b>-3.0%</b>
<b>Operating Expenses</b>	(50,192)	(43,142)	(7,050)	16.3%	(53,554)	3,362	-6.3%
<b>Net Operating Income</b>	10,305	12,024	(1,720)	-14.3%	8,836	1,468	-16.6%
<b>Non-Operating Income (Expense)*</b>	31,525	26,546	4,979	18.8%	30,996	530	1.7%
<b>Net Income Before Depreciation</b>	<b>41,830</b>	<b>38,571</b>	<b>3,259</b>	<b>8.5%</b>	<b>39,832</b>	<b>1,998</b>	<b>5.0%</b>

The month of May saw continued volatility in the US equity markets, with major indices ending effectively flat following a month of ups and downs. The May Consumer Price Index (CPI) report showed prices having increased 0.1% from April, and 4.0% over the previous 12 months. Additionally, personal consumption expenditures only rose 0.1% in May, suggesting the Federal Reserve's increased rates and tightening monetary policy may be taking effect. The national unemployment rate rose to 3.7%, matching its highest rate since March 2022, and the economy added 339,000 jobs. In Washoe County, the unemployment rate rose from 4.0% to 4.1%.

RNO continues to display increased passenger traffic year over year with 183,100 enplanements in May 2023, an increase of 4.9% from May 2022. While demand for travel in the Reno-Tahoe region remains strong, airlines continue to have difficulty with staffing as unemployment remains at historic lows. The resulting reduction in announced airline traffic can have an impact on both airline and non-airline revenues.

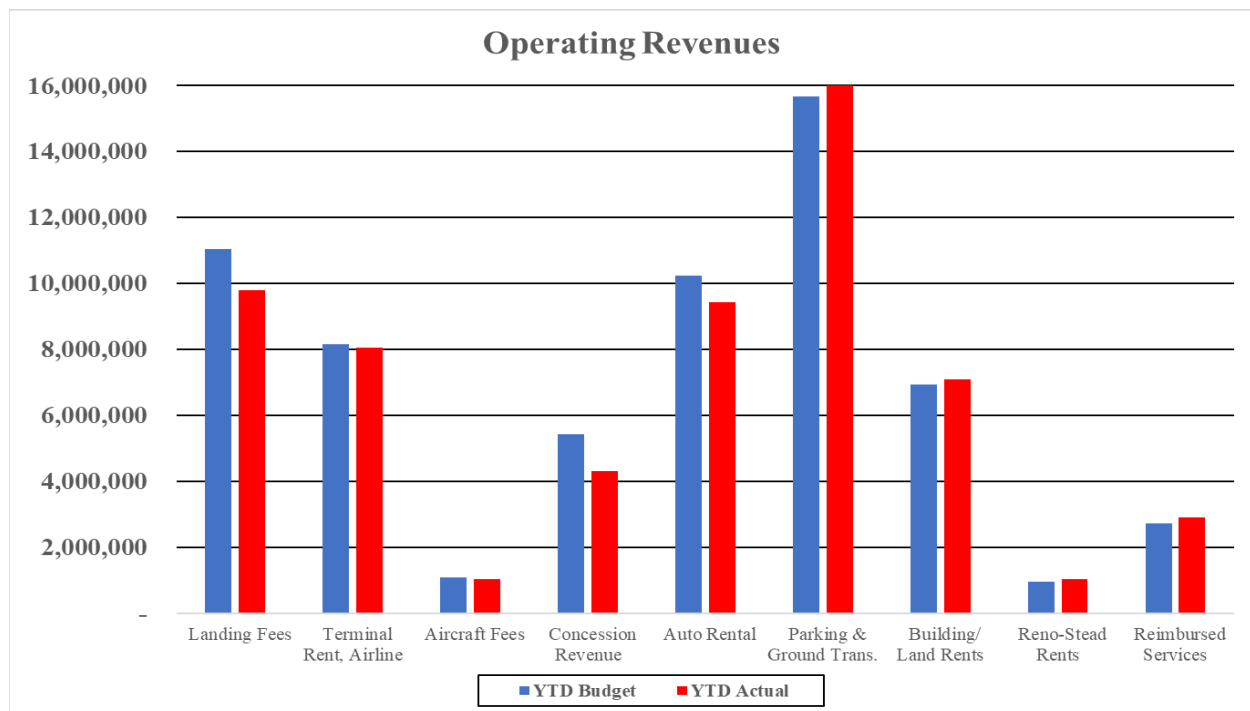


Based on actual results through May 31, 2023, net income before depreciation was approximately \$41.830 million, a \$1.998 million or 5.0% increase from our budget forecast. This is primarily due to operating expenses coming in \$3.362 million below budget. Non-Operating revenues include \$12.608 million in federal stimulus funds (CARES, ARPA) through May 2023. The majority of federal stimulus was to pay for debt service and personnel expenses in the Airfield cost center. \$21,000 ARPA funds were rolled out in the month of May to offset some concessionaires' space rents and concession fees.

## TOTAL OPERATING REVENUES

Total operating revenue for the first 11 months of FY 2022-23 is \$60.497 million, \$1.893 million or 3.0% less than the budget forecast, and \$5.330 million or 9.7% greater than operating revenues for the same period in FY 2021-22. Airline revenue is \$1.374 million or 7.1% below budget primarily due to the lower than anticipated landed weight reported by all major airlines and exit of aha! and Frontier Airlines from the Reno-Tahoe market.

Non-airline revenue is \$518,900 or 1.2% below budget, primarily due to the ARPA fund rollout to concessionaires, a softness in the car rental market, and lower than forecasted passenger traffic. ARPA funds are recognized as non-operating income per FAA guidelines, therefore, reducing non-airline operating revenues. There is no impact on the overall cash received by RTAA. Non-airline revenues include parking and ground transportation, auto rental, retail, food and beverage concessions, gaming revenues, advertising, terminal rents, and other building rents. The chart below reflects actual operating revenues for the fiscal year as compared to the budget amount.



## AIRLINE REVENUES

Airline revenues are collected in accordance with rates and charges as specified by the formula in RTAA's Airline-Airport Use and Lease Agreement (AAULA) with the signatory airlines effective July 1, 2015, through June 30, 2023. RNO has seen a reduction in the current year airline activity due to

schedule changes impacted by labor shortages and increased operating costs. Aha! Airlines filed for bankruptcy in August 2022 and ceased operations at RNO. In addition, Frontier Airlines terminated operations on February 14, 2023, contributing to the landed weight deficit. These impacts are partially offset by new scheduled service provided by Spirit and Sun Country airlines.

### Landing Fees

The formula for calculating landing fees consists mostly of cost recovery of Airfield related operating and capital improvement costs offset by Airfield derived revenues. Landing fees were budgeted and collected at \$3.50 per 1,000 lbs. of landed weight. Based on the actual results in the first 11 months of FY 2022-23, the calculated landing fee per 1,000 lbs. was \$3.75. The calculation includes \$950,000 of federal stimulus funds applied as a credit to reduce landing fees.

FY 2022-23 total landing fee revenues are \$9.801 million, \$1.258 million or 11.4% below the budgeted amount. The decrease is due to lower landed weight reported by Aha!, Alaska, Allegiant, American, Delta, Frontier, JetBlue, Southwest, Spirit, United, FedEx, and UPS, partially offset by higher landed weight reported by Volaris, Sun Country, and other non-signatory airlines. Landed weight through May is 0.4% greater than the same period in FY 2021-22. While below budget, FY 2022-23 monthly landed weights have been consistently above FY 2021-22 since November 2022.

### Airline Terminal Rents

Airline terminal rents reflect cost recovery of Terminal costs allocated to airline occupied facilities with total facility costs divided by rentable terminal square footage. The budgeted average signatory rental rate is \$54.40 per sq. ft. per annum. Based on actual results for the eleven-month period ending May 31, 2023, airline terminal rental revenues were \$8.048 million, approximately \$116,800 or 1.4% below budget.

## **NON-AIRLINE REVENUES**

Non-airline operating revenues are critical for RTAA to meet operating costs outside of airline affiliated operations. While airline revenues are collected as cost recovery for airline affiliated operations, non-airline operating revenues facilitate internal operations, equipment acquisitions, and capital improvement projects that are not directly associated with airline operations. Non-airline operating revenues are primarily comprised of terminal and rental car concession revenues, public parking, building/land rents, and reimbursement of RTAA provided services. Based on actual results for the eleven-month period ending May 31, 2023, non-airline operating revenues totaled \$42.648 million, \$4.413 million or 11.5% greater than the same period in FY 2022-23, and \$518,900 or 1.2% less than the budgeted amount.

As previously mentioned, the application of ARPA funds to offset concessionaire rents and revenues are not considered operating revenues, but non-operating revenues. As such, the \$1.584 million of ARPA funds rolled out fiscal year-to-date have been backed out of this total, resulting in the below budget tally. Disregarding the ARPA credit, non-airline operating revenues are \$44.232 million, \$1.065 million or 2.5% over budget.

All minimum annual guarantees have been reinstated for FY 2022-23, although have not been applicable to date with the increase in passenger traffic. Additionally, we have seen substantial growth in parking and ground transportation, as well as non-airline rents at both RNO and RTS. Parking and ground transportation revenues are up \$3.930 million or 30.6% when compared to the same period in FY 2021-22, RNO building and land rents are up \$437,000, or 6.6% year over year, and Reno-Stead rents are up \$136,900, or 15.3% year over year.

## NON-OPERATING REVENUES

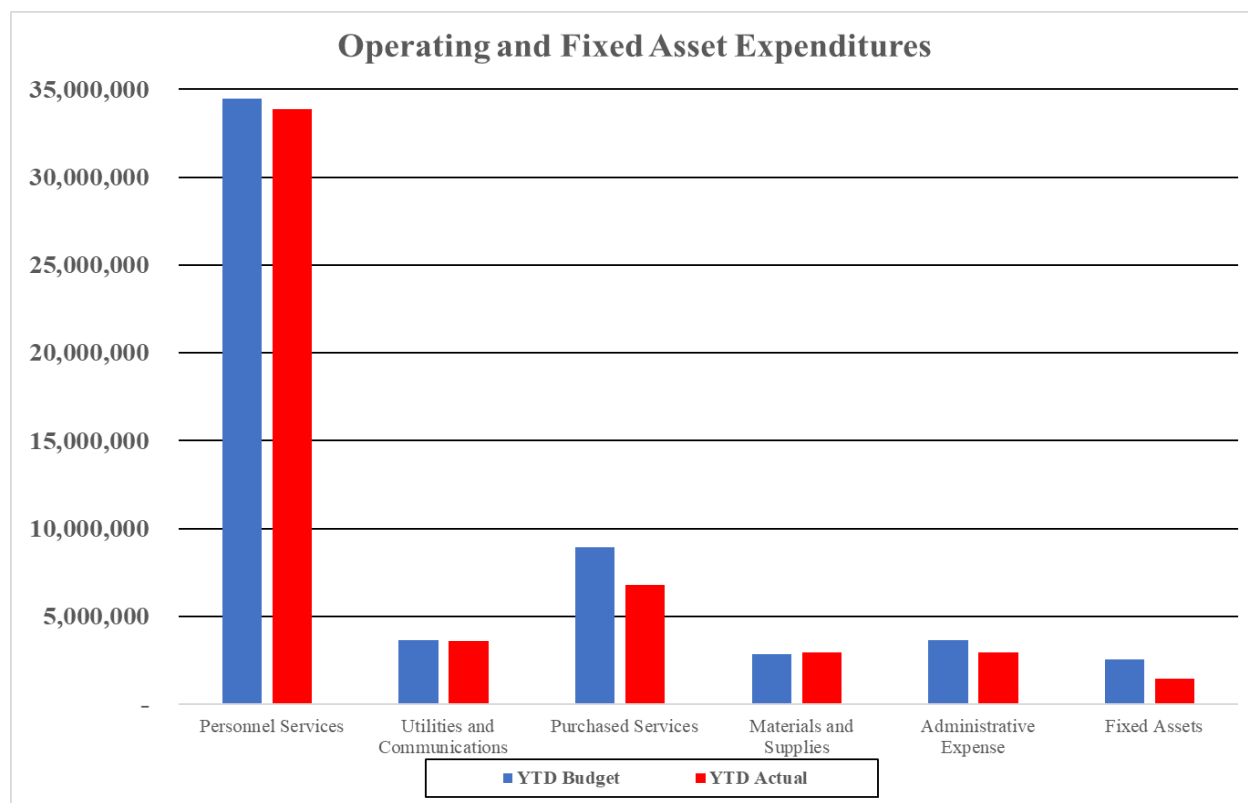
Non-operating revenues of \$31.525 million through May 31, 2023, are \$4.979 million or 18.8% greater than the same period in FY 2021-22, and \$529,600 or 1.7% greater than the budget forecast. This revenue category is primarily comprised of Customer Facility Charges (CFCs) associated with the rental car activity, Passenger Facility Charges (PFCs), federal stimulus funds (CARES, ARPA), interest income, and aviation fuel tax.

CFCs were increased from \$5.50 to \$6.50 per transaction day effective July 1, 2022. Due to the operating procedures of the car rental companies, rental car agreements booked prior to the announced new CFC rates are quoted and collected at the previous rates. This timing issue was not reflected in the adopted budget forecast. Additionally, while higher than the previous year, the below budget passenger traffic directly impacts CFC collections. Furthermore, as inflation has remained sticky, passengers have decided to choose more cost-effective means of transportation, as reflected in the increased Parking and Ground Transportation revenue. CFC revenues are \$1.297 million or 22.6% greater than the same period last year, and \$2.649 million or 27.4% under budget.

PFC revenues are collected by airlines on ticket sales at \$4.50 per enplaned passenger, with RTAA receiving a net of \$4.39 after an \$0.11 administration fee. PFC revenues of \$8.002 million through May 31, 2023, are \$194,200 or 2.5% greater than the same period in FY 2021-22, and \$138,000 or 1.7% below budget. The shortfall in PFC revenue compared to budget is directly attributable to the reduction in scheduled airline activity by the airlines.

## OPERATING EXPENSES

Operating expenses through the first 11 months of FY 2022-23 are \$50.192 million, \$3.362 million or 6.3% below budget, and \$7.050 million or 16.3% greater than the previous fiscal year through the same period. Operating expenses are grouped into five categories: Personnel Services, Utilities and Communications, Purchased Services, Materials and Supplies and Administrative expenses. Of the five, only Materials and Supplies are over budget, at \$106,600 or 3.7% over forecast due to the higher consumption rates of supplies, seasonality of some purchases, and the increase of prices overall due to inflation. Personnel Services are \$579,300 or 1.7% below budget, Utilities and Communications \$69,300 or 1.9%, Purchased Services \$2.121 million or 23.7%, and Administrative Expenses \$698,200 or 19.2% below budget. Vacant positions are the key driver of below budget personnel services, as Washoe County continues to see a historically low unemployment rate of 4.1% in May. The timing of consulting services, lower costs associated with contracted services, and lower repair and maintenance cost contributed to the significant savings in Purchased Services. The lower Administrative Expenses are due to less training and travel through May.



## DEBT SERVICE

On July 14, 2022, the Board approved a \$50 million non-revolving credit agreement with Wells Fargo Bank to provide short-term financing for RTAA's capital program. Funds may be drawn in any amount not to exceed \$50 million. As of May 31, 2023, \$15.104 million has been utilized leaving a balance of \$34.896 million available for future RTAA capital financing requirements. Repayment of drawn amounts, plus accrued interest and fees is currently being repaid from federal stimulus funds.

## KEY BENCHMARKS

The following are key benchmarks and ratios used to measure financial activities and monitor the financial health and condition of RTAA:

Key Statistics / Benchmarks	YEAR TO DATE (May 31, 2023)						
					91.7% Of Fiscal Year		
	CURRENT YEAR	PRIOR YEAR	VARIANCE	%	Y-T-D BUDGET	VARIANCE	%
Enplaned Passengers	2,025,802	1,888,573	137,229	7.3%	2,163,125	(137,323)	-6.3%
Airline Cost Per Enplaned Passenger	\$ 6.83	\$ 5.55	\$ 1.28	23.1%	\$ 7.48	(0.65)	-8.7%
Non-Airline Revenues per EPAX (a)	\$ 19.62	\$ 18.86	\$ 0.76	4.0%	\$ 18.69	0.92	4.9%
Operating Ratio	83.0%	78.2%	4.8%	6.1%	85.8%	-2.9%	-3.3%
Days Cash On Hand	492	552	(59.2)	-10.7%	451	41.8	9.3%
Federal Stimulus Funds	\$ 12,608,214	\$ 12,957,955	\$ (349,741)	-2.7%	\$12,512,775	95,439	0.8%

(a) Excludes cost reimbursement for the Baggage Handling System (BHS) paid by the airlines.



### Enplaned Passengers

Enplaned passengers through the eleven-period ending May 31, 2023, were 2.026 million, 137,200 or 7.3% greater than enplaned passengers through the same period in FY2021-22, and 137,300 or 6.3% below the budget forecast to date. Enplaned passengers are critical to non-airline revenues, such as public parking, rental car concessions, food and beverage concessions, gaming, etc. Passenger traffic in FY 2022-23 has been higher than the same period of the prior year every month since November 2022, exceeding pre-pandemic levels. However, while enplaned passengers are at record levels, they continue to come in below budget as airlines have reduced schedules coupled with the impacts of terminated operations at RNO by aha! and Frontier airlines.

### Airline Cost per Enplaned Passenger (CPE)

This ratio represents airline payments for use of airport facilities (landing fees and terminal rents) in accordance with the adopted rates and charges methodology as outlined in the airline lease agreement. RTAA targets to maintain a reasonable cost structure for airlines operating at RNO to attract and maintain air service to our community. Due to lower landing weight as airlines have reduced operations, and no federal stimulus funding being applied to offset this reduction in May, the calculated CPE through May is \$6.83 as compared to the FY 2022-23 budget of \$7.51. Pending June airline operations, this number may decrease with the application of the remainder of budgeted stimulus funds.

### Non-Airline Revenue per Enplaned Passenger

This ratio represents operating revenues derived from sources other than the airlines, divided by enplaned passengers for the fiscal year. This financial ratio measures operating revenue capacity from terminal rents, rental car concession fees, public parking, and land and building rents from non-airline facilities at both airports. Through May, Non-Airline Revenues per enplaned passenger was \$19.62 or \$0.92 greater than budget and \$0.76 greater than the same period of the prior year. Parking and ground transportation revenue have exceeded budget contributing to the \$0.92 increase over budget despite the lower than forecasted passenger traffic due to reduced airline operations. Another contributing factor was \$1.584 million of ARPA concession relief credits that offset concessionaire rents and revenues.

### Operating Ratio

The Operating Ratio is calculated by dividing operating and maintenance expenses by total operating revenues. This ratio indicates whether the level of operating expenses as a proportion of operating revenues are consistent and tracking with the approved expenditures and revenues adopted in the budget. Generally, a lower ratio of expenses to revenues is positive since it reflects an improvement in the net operating revenues available to pay debt service and generate additional cash flow. Through May, the operating ratio equals 83.0%, or 4.8% higher than the same period of the prior year and 3.3% less than the current year budget forecast. This means RTAA is operating 3.3% more efficiently than forecast. The reason for this efficiency is largely due to reduced operating costs and higher than forecast parking and ground transportation revenues offsetting lower than expected passenger activity.

### Days Cash on Hand (DCOH)

DCOH is an important measure of liquidity. It is calculated by dividing unrestricted cash and investments by the daily operating and maintenance expenditure budget (annual operating and maintenance budget divided by 365 days). As of May 31, 2023, RTAA's DCOH was approximately 492 days, 40 days higher than the previous period. RTAA's policy is a desired target of 365 days. The 2021 median average, as compiled by Moody's Investor Services, is 794 for medium hub airports.

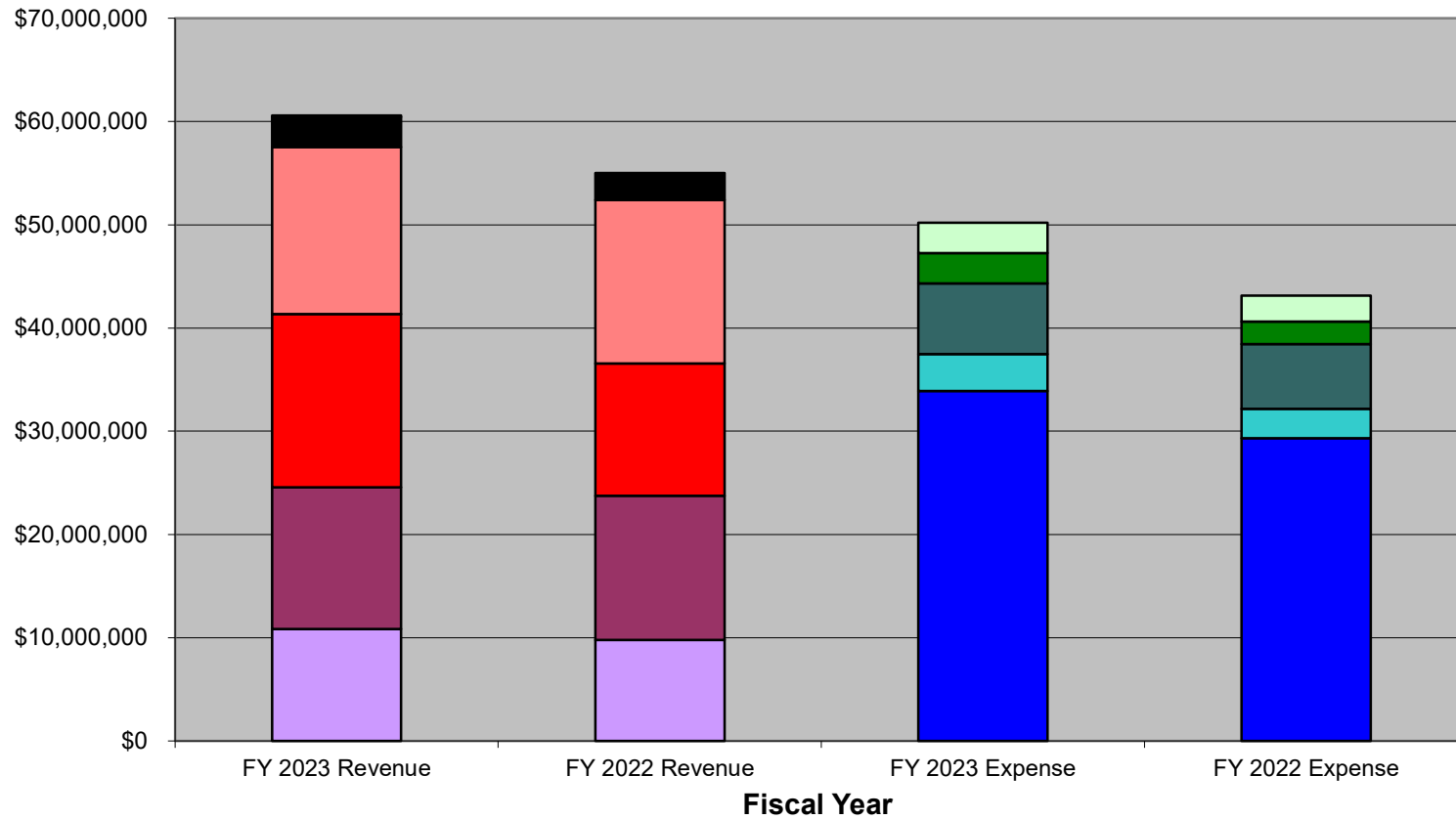
**OPERATING STATEMENT**  
**RENO-TAHOE AIRPORT AUTHORITY**  
For the Eleven Months Ending May 31, 2023

	<i>CURRENT MONTH</i>				<i>For the Eleven Months Ending May 31, 2023</i>						
	CURRENT YEAR	PRIOR YEAR	VARIANCE	%	CURRENT YEAR	PRIOR YEAR	VARIANCE	%	91.67%	OF FISCAL YEAR	
									Y-T-D BUDGET	VARIANCE	%
<b>REVENUES</b>											
Landing Fees	\$ 888,029	\$ 694,507	\$ 193,521	27.9%	\$ 9,800,764	\$ 8,653,809	\$ 1,146,954	13.3%	\$ 11,058,384	\$ (1,257,621)	-11.4%
Terminal Rent, Airline	708,162	690,723	17,438	2.5%	8,048,203	8,278,001	(229,798)	-2.8%	8,165,025	(116,822)	-1.4%
Aircraft Fees	86,432	85,921	511	0.6%	1,049,458	1,133,755	(84,298)	-7.4%	1,079,350	(29,893)	-2.8%
Concession Revenue	543,084	377,199	165,885	44.0%	4,308,615	4,695,735	(387,119)	-8.2%	5,444,176	(1,135,560)	-20.9%
Auto Rental	683,838	753,930	(70,092)	-9.3%	9,425,266	9,245,233	180,033	1.9%	10,242,488	(817,222)	-8.0%
Parking & Ground Transportation	1,659,816	1,345,562	314,254	23.4%	16,758,205	12,828,584	3,929,621	30.6%	15,675,011	1,083,195	6.9%
Reno-Tahoe Building/ Land Rents	642,035	609,645	32,390	5.3%	7,098,224	6,661,255	436,969	6.6%	6,941,483	156,741	2.3%
Reno-Stead Rents	70,036	79,587	(9,551)	-12.0%	1,031,605	894,673	136,931	15.3%	952,783	78,821	8.3%
Reimbursed Services	244,200	211,838	32,362	15.3%	2,910,441	2,624,387	286,054	10.9%	2,731,877	178,564	6.5%
Miscellaneous	19,280	20,917	(1,637)	-8%	66,100	151,212	(85,113)	-56.3%	99,642	(33,542)	-33.7%
<b>OPERATING REVENUE</b>	<b>\$ 5,544,912</b>	<b>\$ 4,869,830</b>	<b>\$ 675,082</b>	<b>13.9%</b>	<b>\$ 60,496,880</b>	<b>\$ 55,166,645</b>	<b>\$ 5,330,235</b>	<b>9.7%</b>	<b>\$ 62,390,219</b>	<b>\$ (1,893,339)</b>	<b>-3.0%</b>
<b>EXPENSES</b>											
Personnel Services	\$ 3,138,242	\$ 2,819,358	\$ 318,884	11.3%	\$ 33,885,752	\$ 29,322,984	\$ 4,562,768	15.6%	\$ 34,465,021	\$ (579,269)	-1.7%
Utilities and Communications	291,258	260,110	31,148	12.0%	3,602,273	2,855,407	746,866	26.2%	3,671,570	(69,297)	-1.9%
Purchased Services	725,505	804,030	(78,525)	-9.8%	6,816,518	6,248,405	568,113	9.1%	8,937,942	(2,121,424)	-23.7%
Materials and Supplies	339,268	242,472	96,796	39.9%	2,952,414	2,218,125	734,289	33.1%	2,845,822	106,592	3.7%
Administrative Expense	320,923	241,102	79,821	33.1%	2,935,169	2,497,282	437,888	17.5%	3,633,397	(698,228)	-19.2%
<b>OPERATING EXPENSES</b>	<b>\$ 4,815,196</b>	<b>\$ 4,367,071</b>	<b>\$ 448,125</b>	<b>10.3%</b>	<b>\$ 50,192,127</b>	<b>\$ 43,142,203</b>	<b>\$ 7,049,924</b>	<b>16.3%</b>	<b>\$ 53,553,753</b>	<b>\$ (3,361,626)</b>	<b>-6.3%</b>
<b>NET OPERATING INC. BEFORE DEPR.</b>	<b>\$ 729,716</b>	<b>\$ 502,759</b>	<b>\$ 226,956</b>	<b>45.1%</b>	<b>\$ 10,304,753</b>	<b>\$ 12,024,443</b>	<b>\$ (1,719,690)</b>	<b>-14.3%</b>	<b>\$ 8,836,467</b>	<b>\$ 1,468,287</b>	<b>16.6%</b>
Depreciation and Amortization	2,001,845	2,095,950	(94,106)	-4.5%	24,447,030	22,979,967	1,467,063	6.4%	27,498,900	(3,051,871)	-11.1%
<b>OPERATING INCOME</b>	<b>\$ (1,272,129)</b>	<b>\$ (1,593,191)</b>	<b>\$ 321,062</b>	<b>20.2%</b>	<b>\$ (14,142,276)</b>	<b>\$ (10,955,524)</b>	<b>\$ (3,186,752)</b>	<b>-29.1%</b>	<b>\$ (18,662,433)</b>	<b>\$ 4,520,157</b>	<b>24.2%</b>
<b>NON-OPERATING INCOME (EXPENSE)</b>											
Interest Income and G/L on Investments	\$ 192,910	\$ 66,132	\$ 126,778	191.7%	\$ 3,839,505	\$ 176,258	\$ 3,663,247	2078.3%	\$ 805,383	3,034,121	376.7%
Passenger Facility Charge	1,091,214	1,116,312	(25,099)	-2.2%	8,001,591	7,807,378	194,213	2.5%	8,139,542	(137,951)	-1.7%
Customer Facility Charge	663,646	529,212	134,434	25.4%	7,027,070	5,730,050	1,297,020	22.6%	9,675,659	(2,648,589)	-27.4%
Jet Fuel Tax Revenue	23,000	25,051	(2,051)	-8.2%	278,369	259,386	18,983	7.3%	235,308	43,060	18.3%
Federal Stimulus	21,398	0	21,398	n.a.	12,608,214	12,957,955	(349,741)	-2.7%	12,512,775	95,439	0.8%
G/L on Sale of Capital Assets	0	0	0	n.a.	10,871	15,080	(4,209)	-27.9%	0	10,871	n.a.
Other Non-Operating Revenue (Expense)	0	0	0	n.a.	(98,425)	0	(98,425)	n.a.	0	(98,425)	n.a.
Interest Expense	(33,670)	0	(33,670)	n.a.	(141,847)	(399,857)	258,010	-64.5%	(372,885)	231,038	-62.0%
<b>Total</b>	<b>\$ 1,958,498</b>	<b>\$ 1,736,707</b>	<b>\$ 221,791</b>	<b>12.8%</b>	<b>\$ 31,525,346</b>	<b>\$ 26,546,249</b>	<b>\$ 4,979,097</b>	<b>18.8%</b>	<b>\$ 30,995,782</b>	<b>\$ 529,564</b>	<b>1.7%</b>
<b>Net Income Before Capital Contributions</b>	<b>\$ 686,369</b>	<b>\$ 143,516</b>	<b>\$ 542,853</b>	<b>378.3%</b>	<b>\$ 17,383,070</b>	<b>\$ 15,590,725</b>	<b>\$ 1,792,345</b>	<b>11.5%</b>	<b>\$ 12,333,349</b>	<b>\$ 5,049,721</b>	<b>40.9%</b>

**OPERATING STATEMENT**  
**RENO-TAHOE AIRPORT AUTHORITY**  
For the Eleven Months Ending May 31, 2023

	CURRENT MONTH				YEAR TO DATE				ANNUAL BUDGET	
	ACTUAL	BUDGET	VARIANCE		ACTUAL	BUDGET	VARIANCE		TOTAL	ANNUAL BUDGET % TO DATE
			\$	%			\$	%		
<b>REVENUES</b>										
Landing Fees	\$ 888,029	\$ 1,084,215	\$ (196,186)	-18.1%	\$ 9,800,764	\$ 11,058,384	\$ (1,257,621)	-11.4%	\$ 12,102,307	81%
Terminal Rent, Airline	708,162	742,275	(34,113)	-4.6%	\$ 8,048,203	8,165,025	(116,822)	-1.4%	8,907,300	90%
Aircraft Fees	86,432	96,101	(9,669)	-10.1%	\$ 1,049,458	1,079,350	(29,893)	-2.8%	1,181,011	89%
Concession Revenue	543,084	523,327	19,757	3.8%	\$ 4,308,615	5,444,176	(1,135,560)	-20.9%	5,953,000	72%
Auto Rental	683,838	1,003,029	(319,191)	-31.8%	\$ 9,425,266	10,242,488	(817,222)	-8.0%	11,208,806	84%
Parking & Ground Transportation	1,659,816	1,528,171	131,645	8.6%	\$ 16,758,205	15,675,011	1,083,195	6.9%	17,150,500	98%
Reno-Tahoe Building/ Land Rents	642,035	631,044	10,991	1.7%	\$ 7,098,224	6,941,483	156,741	2.3%	7,572,527	94%
Reno-Stead Rents	70,036	86,617	(16,581)	-19.1%	\$ 1,031,605	952,783	78,821	8.3%	1,039,400	99%
Reimbursed Services	244,200	254,958	(10,758)	-4.2%	\$ 2,910,441	2,731,877	178,564	6.5%	2,972,759	98%
Miscellaneous	19,280	9,058	10,222	112.8%	\$ 66,100	99,642	(33,542)	-33.7%	108,700	0%
<b>OPERATING REVENUE</b>	<b>\$ 5,544,912</b>	<b>\$ 5,958,794</b>	<b>\$ (413,882)</b>	<b>-6.9%</b>	<b>\$ 60,496,880</b>	<b>\$ 62,390,219</b>	<b>\$ (1,893,339)</b>	<b>-3.0%</b>	<b>\$ 68,196,310</b>	<b>89%</b>
<b>EXPENSES</b>										
Personnel Services	\$ 3,138,242	\$ 3,101,366	\$ 36,876	1.2%	\$ 33,885,752	\$ 34,465,021	\$ (579,269)	-1.7%	\$ 37,567,907	90%
Utilities and Communications	291,258	354,956	(63,698)	-17.9%	\$ 3,602,273	3,671,570	(69,297)	-1.9%	4,051,370	89%
Purchased Services	725,505	825,023	(99,518)	-12.1%	\$ 6,816,518	8,937,942	(2,121,424)	-23.7%	9,785,852	70%
Materials and Supplies	339,268	269,385	69,883	25.9%	\$ 2,952,414	2,845,822	106,592	3.7%	3,115,322	95%
Administrative Expense	320,923	303,465	17,459	5.8%	\$ 2,935,169	3,633,397	(698,228)	-19.2%	3,985,021	74%
<b>OPERATING EXPENSES</b>	<b>\$ 4,815,196</b>	<b>\$ 4,854,194</b>	<b>\$ (38,998)</b>	<b>-0.8%</b>	<b>\$ 50,192,127</b>	<b>\$ 53,553,753</b>	<b>\$ (3,361,626)</b>	<b>-6.3%</b>	<b>\$ 58,505,471</b>	<b>86%</b>
<b>NET OPERATING INC. BEFORE DEPR.</b>	<b>\$ 729,716</b>	<b>\$ 1,104,600</b>	<b>\$ (374,884)</b>	<b>-33.9%</b>	<b>\$ 10,304,753</b>	<b>\$ 8,836,467</b>	<b>\$ 1,468,287</b>	<b>16.6%</b>	<b>\$ 9,690,839</b>	<b>106%</b>
Depreciation and Amortization	2,001,845	2,500,000	(498,155)	-19.9%	24,447,030	27,500,000	(3,052,971)	-11.1%	30,000,000	81%
<b>OPERATING INCOME</b>	<b>\$ (1,272,129)</b>	<b>\$ (1,395,400)</b>	<b>\$ 123,271</b>	<b>8.8%</b>	<b>\$ (14,142,276)</b>	<b>\$ (18,663,533)</b>	<b>\$ 4,521,257</b>	<b>24.2%</b>	<b>\$ (20,309,161)</b>	<b>70%</b>
<b>NON-OPERTING INCOME (EXPENSE)</b>										
Interest Income and G/L on Investments	\$ 192,910	\$ 73,217	\$ 119,693	163.5%	\$ 3,839,505	\$ 805,383	\$ 3,034,121	376.7%	878,600	437%
Passenger Facility Charge	1,091,214	739,958	351,255	47.5%	\$ 8,001,591	8,139,542	(137,951)	-1.7%	8,879,500	90%
Customer Facility Charge	663,646	947,520	(283,874)	-30.0%	\$ 7,027,070	9,675,659	(2,648,589)	-27.4%	10,588,500	66%
Jet Fuel Tax Revenue	23,000	21,392	1,608	7.5%	\$ 278,369	235,308	43,060	18.3%	256,700	108%
Federal Stimulus	21,398	1,137,525	(1,116,127)	-98.1%	\$ 12,608,214	12,512,775	95,439	0.8%	13,650,300	92%
G/L on Investments and Sale of Assets	0	0	0	n.a.	\$ 10,871	0	10,871	n.a.	0	n.a.
Other Non-Operating Revenue (Expense)	0	0	0	n.a.	(98,425)	0	(98,425)	n.a.	0	n.a.
Interest Expense	(33,670)	(33,900)	230	-0.7%	\$ (141,847)	(372,885)	231,038	-62.0%	(406,800)	35%
<b>Total</b>	<b>\$ 1,958,498</b>	<b>\$ 2,885,712</b>	<b>\$ (927,214)</b>	<b>-32.1%</b>	<b>\$ 31,525,346</b>	<b>\$ 30,995,782</b>	<b>\$ 529,564</b>	<b>1.7%</b>	<b>\$ 33,846,800</b>	<b>93%</b>
<b>Net Income Before Capital Contributions</b>	<b>\$ 686,369</b>	<b>\$ 1,490,312</b>	<b>\$ (803,943)</b>	<b>-53.9%</b>	<b>\$ 17,383,070</b>	<b>\$ 12,332,249</b>	<b>\$ 5,050,821</b>	<b>41.0%</b>	<b>\$ 13,537,639</b>	<b>128%</b>

## Operating Revenue and Expense YTD through May 31, 2023





# SUMMARY OF NON-AIRLINE REVENUES

Reno-Tahoe Airport Authority

	5/31/2023 YTD Actual	5/31/2022 YTD Actual	Over (Under) Prior Year	% Variance	5/31/2023 Year to Date Budget	Over (Under) Budget	% Variance	2022-23 Annual Budget	% of Annual Budget
Aircraft Fees - Reno	\$ 1,030,029	\$ 1,062,159	\$ (32,130)	-3.0%	1,029,152	\$ 877	0.1%	1,122,711	91.7%
Aircraft Fees - Stead	19,428	71,597	(52,168)	-72.9%	50,198	(30,770)	-61.3%	58,300	33.3%
Gaming Concession	937,639	1,160,569	(222,930)	-19.2%	1,096,090	(158,452)	-14.5%	1,199,500	78.2%
Food & Beverage	1,243,936	1,339,771	(95,836)	-7.2%	1,927,090	(683,155)	-35.5%	2,108,900	59.0%
Retail/Merchandise	594,361	769,841	(175,480)	-22.8%	1,023,170	(428,809)	-41.9%	1,119,700	53.1%
Advertising	698,046	671,303	26,742	4.0%	776,783	(78,738)	-10.1%	847,400	82.4%
Other Concessions	96,876	183,731	(86,855)	-47.3%	132,183	(35,307)	-26.7%	144,200	67.2%
FBO and Ground Handlers	700,385	549,073	151,312	27.6%	470,250	230,135	48.9%	513,000	136.5%
Stead Concessions	37,373	21,447	15,926	74.3%	18,608	18,765	100.8%	20,300	184.1%
Auto Rental	9,425,266	9,245,233	180,033	1.9%	10,242,488	(817,222)	-8.0%	11,208,806	84.1%
Ground Transportation	614,249	284,344	329,905	116.0%	633,050	(18,802)	-3.0%	690,600	88.9%
Auto Parking	16,143,957	12,544,241	3,599,716	28.7%	15,041,960	1,101,996	7.3%	16,459,900	98.1%
Other Terminal Rents	768,875	759,803	9,073	1.2%	725,450	43,425	6.0%	791,400	97.2%
Reno-Tahoe Building Rents	3,077,608	2,864,087	213,522	7.5%	2,926,742	150,866	5.2%	3,192,810	96.4%
Reno-Tahoe Land Rents	3,251,740	3,037,365	214,375	7.1%	3,289,291	(37,551)	-1.1%	3,588,318	90.6%
Reno-Stead Rents	1,031,605	894,673	136,931	15.3%	952,783	78,821	8.3%	1,039,400	99.3%
Reimbursed Services	2,910,441	2,624,387	286,054	10.9%	2,731,877	178,564	6.5%	2,972,759	97.9%
Miscellaneous	66,100	151,212	(85,113)	-56.3%	99,642	(33,542)	-33.7%	108,700	60.8%
Total Non-Airline Operating Revenue	42,647,913	38,234,835	4,413,078	11.5%	43,166,810	(518,896)	-1.2%	47,186,704	90.4%
Non Operating Revenue (a)	1,783,296	727,452	1,055,844	145.1%	1,040,692	742,604	71.4%	1,135,300	157.1%
<b>TOTAL NON-AIRLINE REVENUE</b>	<b>\$ 44,431,209</b>	<b>\$ 38,962,287</b>	<b>\$ 5,468,922</b>	<b>14.0%</b>	<b>\$ 44,207,501</b>	<b>\$ 223,708</b>	<b>0.5%</b>	<b>\$ 48,322,004</b>	<b>91.9%</b>
						(518,896)			
Year to Date Enplaned Passengers	2,025,802	1,888,573			2,163,125			2,367,203	
Non-Airline Revenue Per EPAX (b)	<b>\$ 19.62</b>	<b>\$ 18.86</b>			<b>\$ 18.69</b>			<b>\$ 18.68</b>	
Non-Airline Revenue Per EPAX (c)	<b>\$ 10.79</b>	<b>\$ 11.38</b>			<b>\$ 10.90</b>			<b>\$ 10.89</b>	

(a) Excludes PFC and CFC revenues

(b) Total Non-Airline Revenue less Reimbursed Services divided by enplaned passengers

(c) Non-Airline Revenue (Concessions, Rental Car, Other Rents) Per Enplaned Passenger (Strategic Plan Measure)

## NET REVENUE SHARING - YEAR TO DATE

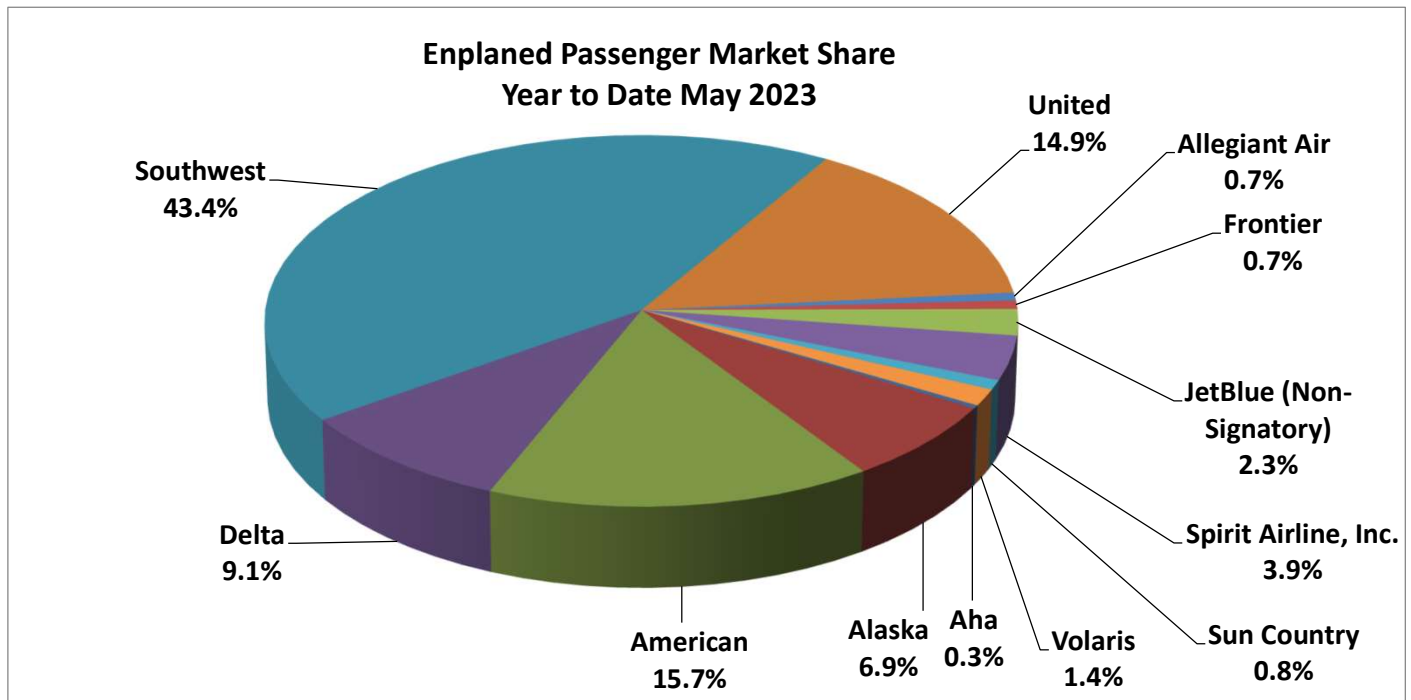
Reno-Tahoe Airport Authority  
For the Eleven Months Ending May 31, 2023

	<u>Airfield</u>	<u>Terminal</u>	<u>Baggage System</u>	<u>Landside</u>	<u>Other</u>	<u>Reno Stead</u>	<u>Total</u>
Revenue Sharing							
Airline Revenue	10,629,721	12,628,485	-	-	-	-	23,258,205
Non Airline Revenue	1,056,110	5,530,441	1,719,732	26,183,471	8,758,289	1,088,406	44,336,450
<b>Total Revenue</b>	<b>11,685,830</b>	<b>18,158,926</b>	<b>1,719,732</b>	<b>26,183,471</b>	<b>8,758,289</b>	<b>1,088,406</b>	<b>67,594,655</b>
<b>Budgeted Revenue</b>	<b>12,306,266</b>	<b>19,594,758</b>	<b>1,701,295</b>	<b>26,163,139</b>	<b>7,416,408</b>	<b>1,024,833</b>	<b>68,206,699</b>
O&M Expense	\$ 11,725,880	\$ 20,927,078	\$ 1,750,962	\$ 8,959,495	\$ 4,204,990	\$ 2,128,563	\$ 49,696,968
Debt Service	-	-	-	-	7,768,758	-	7,768,758
Pre Bond Loan	-	-	-	-	-	-	-
O&M Reserve	250,920	436,470	33,610	207,975	94,355	46,932	1,070,262
Fixed Asset	501,519	369,938	-	221,380	135,801	48,661	1,277,299
Capital Project	131,431	541,015	-	24,623	348,535	64,102	1,109,706
Amort. Capital Items	1,105,915	354,394	-	974,522	619,306	388,434	3,442,571
Special Fund	-	384,840	-	-	-	-	384,840
Federal Stimulus	(950,000)	-	-	-	(7,711,412)	-	(8,661,412)
<b>Total Requirement</b>	<b>12,765,665</b>	<b>23,013,735</b>	<b>1,784,572</b>	<b>10,387,995</b>	<b>5,460,332</b>	<b>2,676,692</b>	<b>56,088,991</b>
<b>Budgeted Requirement</b>	<b>13,413,276</b>	<b>24,116,349</b>	<b>1,701,295</b>	<b>11,093,634</b>	<b>5,389,273</b>	<b>2,876,790</b>	<b>58,590,617</b>
<b>Net Revenues</b>	<b>(1,079,835)</b>	<b>(4,854,809)</b>	<b>(64,840)</b>	<b>15,795,476</b>	<b>3,297,957</b>	<b>(1,588,286)</b>	<b>11,505,664</b>
<b>Budgeted Net Revenues</b>	<b>(1,107,010)</b>	<b>(4,521,591)</b>	<b>-</b>	<b>15,069,505</b>	<b>2,027,135</b>	<b>(1,851,957)</b>	<b>9,616,082</b>
Months	11					Airport Share	\$ 5,752,832
						Airline Share	\$ 5,752,832

# SUMMARY OF ENPLANED PASSENGERS BY AIRLINE

Reno-Tahoe International Airport

Enplaned passengers by Airline	Month			Year-to-date		
	May-23	May-22	Percent change	YTD 2022-23	YTD 2021-22	Percent change
<b>Major/national carriers (Signatory)</b>						
Aha!	0	2,019	-100.0%	5,407	14,507	-62.7%
Alaska	14,548	16,943	-14.1%	140,441	180,016	-22.0%
American	29,965	26,404	13.5%	317,709	335,224	-5.2%
Delta	18,199	17,391	4.6%	183,366	199,435	-8.1%
Southwest	81,190	73,220	10.9%	878,395	752,914	16.7%
United	23,141	26,844	-13.8%	300,946	260,540	15.5%
<b>Total</b>	<b>167,043</b>	<b>162,821</b>	<b>2.6%</b>	<b>1,826,264</b>	<b>1,742,636</b>	<b>4.8%</b>
<b>Non-Signatory and Charter</b>						
Allegiant Air	1,079	2,551	-57.7%	13,877	30,610	-54.7%
Frontier	0	1,810	-100.0%	14,980	31,102	-51.8%
JetBlue	2,637	4,409	-40.2%	46,152	50,418	-8.5%
Spirit Airlines	8,332	0	n.a.	78,239	0	n.a.
Sun Country Airlines	1,499	986	52.0%	17,063	2,353	625.2%
Volaris	2,522	1,910	32.0%	29,227	24,140	21.1%
Other Charters	0	0	n.a.	0	7,314	-100.0%
<b>Total</b>	<b>16,069</b>	<b>11,666</b>	<b>37.7%</b>	<b>199,538</b>	<b>145,937</b>	<b>36.7%</b>
<b>Total enplaned passengers</b>	<b>183,112</b>	<b>174,487</b>	<b>4.9%</b>	<b>2,025,802</b>	<b>1,888,573</b>	<b>7.3%</b>



# RTAA Liquidity Position

